



PLANNING COMMISSION MEETING AGENDA
TUESDAY FEBRUARY 21, 2023

7:30 PM REGULAR MEETING

1. Call to Order
2. Roll Call
3. Approval of Minutes:
 - a. November 15, 2022 Planning Commission Meeting Minutes.
 - b. January 17, 2023 Planning Commission Meeting Minutes.
 - c. February 9, 2022, City Council Meeting Minutes (For Information Only)
4. **Continued from January 17, 2023 Meeting.** William Stoddard (Applicant) and John Zeglin (Owner) requests that the City consider the following actions for the property located at 9285 US Hwy 12, Independence, MN (PID No. 18-118-24-21-0001):
 - a. A comprehensive plan amendment to allow a portion of the property to be re-guided to the Urban Commercial - UC zoning district. A portion of the property is already guided Urban Commercial.
 - b. Rezoning of a portion of the property from AG-Agriculture to UC-Urban Commercial.
 - c. Conditional Use Permit to allow a planned unit commercial development on the subject property. The proposed development would include office warehouse and private garage condominium buildings.
 - d. Site Plan Review to allow the proposed development.
 - e. Preliminary Plat to allow the subdivision of the property into eight (8) residential lots and associated lots to allow the proposed commercial development.
5. Open/Misc.
6. Adjourn.

MINUTES OF A MEETING OF THE
INDEPENDENCE PLANNING COMMISSION
TUESDAY, NOVEMBER 15, 2022, AT 7:30 PM

1. CALL TO ORDER

Pursuant to due call and notice thereof, a work session of the Independence Planning Commission was called to order by Chair Gardner at 7:30 p.m.

2. ROLL CALL:

PRESENT: Commissioners Gardner (Chair), Story, Usset, & Dumas (online).
ABSENT: Volkenant, Thompson, and Tearse
STAFF: City Administrator Kaltsas
VISITORS: See Sign-In Sheet

3. APPROVAL OF MINUTES:

- a. October 18, 2022, Planning Commission Meeting
- b. November 1, 2022, City Council Meeting Minutes (For Information Only)

Motion by Story to approve the October 18, 2022, Planning Commission minutes, seconded by Usset. Ayes: Gardner, Story, Usset, & Dumas (online). Nays: None. Absent: Volkenant, Thompson, and Tearse. Abstain: None. Motion Approved.

4. **PUBLIC HEARING:** Ryan Nash (Applicant/Owner) is requesting the following action for the properties located at 2870 Lake Sarah Road (PID No's. 16-118-24-13-0003 and 16-118-24-42-0001) in the City of Independence, MN:

- a. A minor subdivision to allow a lot combination that would create one parcel from the two existing parcels with a combined acreage of 5.4 acres.

Request:

Ryan Nash (Applicant/Owner) is requesting the following action for the properties located at 2870 Lake Sarah Rd. (PID No's. 16-118-24-13-0003 and 16-118-24-42-0001) in the City of Independence, MN:

- a. A minor subdivision to allow a lot combination that would create one parcel from the two existing parcels with a combined acreage of 5.4 acres.

Property/Site Information:

There are two subject properties being considered. Both properties are located on the south side of Lake Sarah Rd., north of Highway 12 and north of the BNSF railroad. The applicant owns both properties and has an existing home and two detached accessory structures on the north

property. The north property has frontage on Lake Sarah Rd. and the south property has no public road frontage but does abut the BNSF railroad right of way. The property has the following site characteristics:

Property Information: 2870 Lake Sarah Road

Zoning: *Agriculture*

Comprehensive Plan: *Agriculture*

Acreage (Before): *PID No. 16-118-24-13-0003 2 acres*

PID No. 16-118-24-42-0001 3.4 acres

Acreage (After): *5.4 acres*

Discussion:

The applicant approached the City about the possibility of adding onto the existing detached accessory building located on the north parcel. The City determined that the north property does not have enough area to support an expansion of the existing detached accessory structure. It was noted that the north and south properties could be combined to allow for the proposed building expansion.

The existing detached accessory structure is 1,200 square feet (30' x 40'). The applicant is proposing to add a 640 square foot addition to the north side of the building and a 400 square foot open addition to the south side. The total square footage of the existing structure, plus the proposed accessory structure expansion is 2,240. The City allows properties that are less than 2.5 acres to have a maximum square footage of 1,850. For properties greater than 2.5 acres, the City allows a maximum square footage of detached accessory structures based on a calculation of 2% of the buildable upland.

The north property is 2 acres, and the south property is 3.4 acres. It is estimated that there is 2.22 acres of wetlands on the south property. By combining the properties, the applicant would be able to realize approximately 2,770 square feet of detached accessory structures as follows:

Combined Property:	5.4 acres
<u>Wetland Acres:</u>	<u>2.14 acres</u>
Upland Acres:	3.18 acres
Buildable Upland:	138,521 SF
Allowed Accessory Structure:	2,770 SF
Proposed Accessory Structure SF:	2,240 SF

There are several items that should be noted by the City during consideration of the application:

1. The south lot does not have any public frontage.
2. The existing detached accessory building meets applicable building setbacks. The proposed expansion of the structure will meet all applicable setbacks in the after condition.
3. The requisite drainage and utility easements would need to be dedicated to the City. The applicant shall execute the necessary documents to convey the easements as determined necessary by the City.

The proposed subdivision to allow a lot combination appears to meet all applicable standards of the City's zoning and subdivision ordinance. The combined lot will fit into the surrounding area and have minimal impacts on the surrounding properties.

Neighbor Comments:

The City has not received any written comments regarding the proposed minor subdivision.

Recommendation:

Staff is seeking a recommendation from the Planning Commission for the requested minor subdivision with the following findings:

1. The proposed subdivision allowing a lot combination meets all applicable criteria and conditions stated in Chapter V, Section 500, Planning and Land Use Regulations of the City of Independence Zoning Ordinance.
2. The applicant shall submit a separate exhibit showing the after conditions prior to City Council consideration of the minor subdivision.
3. The applicant shall dedicate the requisite drainage and utility easements to the City. The applicant shall execute the necessary documents to convey the easements as determined necessary by the City.
4. The applicant shall pay for all costs associated with the City's review of the requested minor subdivision.
5. The applicant shall record the subdivision and City Council Resolution with Hennepin County within six (6) months of approval.
6. The applicant shall execute and record the requisite drainage and utility easements with Hennepin County within six (6) months of approval.

Kaltsas reviewed the Applicant's/Owner's request (pkt. pages 11-18) for a minor subdivision to allow a lot combination that would create one parcel from the two existing parcels with a combined acreage of 5.4 acres for the properties located at 2870 Lake Sarah Rd, Independence, MN (PID No's. 16-118-24-13-0003 and 16-118-24-42-0001). The applicant wants to expand his detached accessory structure (shed) but because his North lot is less than 2.5 acres, it is limited to the 2% buildable upland and thus the proposed addition's sqft puts it over the allowable 1,850 sq footage. He owns 2 lots and could combine them to then access his buildable upland.

For detached accessory structures, 2% buildable upland is allowed. Without doing a full wetland delineation for the allowance, we noted that he could expand existing detached accessory 1,200 SF (30' x 40') structure with the proposed 640 SF addition to the north side of the building and a 400 SF open addition to the south side. The proposed addition adds another 1,040 SF, so the total accessory structure is 2,240 SF. Properties less than 2.5 acres automatically get 1,850 SF (regardless of wetland) which exceeds than the 1,850 SF allowed. If lot is greater than 2.5 acres (which the AFTER would be 5.4 acres), the Buildable Upland is used, which would be 2.14 estimated Wetland acres, leaving 3.18 Upland acres and 2,770 SF Allowable Detached Accessory Structure.

The City noted several items: If the applicant wanted to do another building / expansion, the City would likely require a Wetland delineation, although the current buffer suffices for this project. The south lot does not have any public frontage and difficult to develop on its own. Combining the 2 properties would create a 5.4-acre parcel and is straight forward. No other jurisdictions (school district, watershed, etc.) seem to create a property line and they were just 2 separate parcels. The proposed survey shows required 10' drainage utility easements around the perimeter, and none are needed to vacate which is normal for subdivision or combining of properties.

Gardner opened Public Hearing. (No one present.)
Motion by Story, seconded by Usset to close Public Hearing.

Gardner- Section 14 & 16 property line

Dumas- How many orphan parcels are there?

Kaltsas- Not many. Most are divided by a taxing jurisdiction change, usually by school district or municipal boundaries. When going into the plat, they can often be one but will have 2 PIDs. This isn't like that. It's in the same. This will be nice to clean up that issue.

Gardner – What about the lot width to length ratio? It's 165' wide.

Kaltsas – This will exceed lot width-length ratio, but it won't be an issue because we're taking 2 non-conforming lots. If creating a new lot, we'd create a variance waiver.

Gardner – What's the legal definition of a lean-to?

Kaltsas- We consider it to be structure. It's a permanently affixed structure, open or closed, with a roof and footings. Lean-tos often become sided and then enclosed. If one has a 10'x20' movable/non-permanent, open-sided horse shelters, these are not included in the allowable SF total.

Nash, applicant- (Not recorded at microphone) Kaltsas will change the report to clarify going forward. He had thought it was after the fact since it was in the permit applications.

Gardner – Combining the lots will fix the problems. Was the 2% upland changed in 2019?

Kaltsas- I think it might have been changed in 2012-2014. Making it wholly proportional fixed it. 1,600 SF before was the max size. Most got a bump to what was allowed, and we didn't take away anything from people. Our shed permitting has really risen.

Motion by Story, second by Usset for applicant Ryan Nash regarding property at 2870 Lake Sarah Road, Independence Road (PID No's. 16-118-24-13-0003 and 16-118-24-42-0001) following staff's recommendations to allow adjoining of these properties. Ayes: Gardner, Story, Usset, & Dumas (online). Nays: None. Absent: Volkenant, Thompson, and Tearse. Abstain: None. Motion Approved. 4:0

Kaltsas said that nothing had come to the City regarding this request. This will go on the December 6th City Council meeting. Other items will be the Business Park on Nelson Rd and another lot combination on Copeland Rd.

5. Open/Misc.

Upcoming City items:

- Because there have been properties being sold without septic compliance, the City is looking to change the ordinance to require having an escrow upon closing to help with that.
- Barndominiums: The City has been getting lots of barndominium requests, and we will bring this before the Planning Commission. So put on your barndominium hat when you go out around the

city because I think we will try to address it in January. A lot of people are buying land then coming in wanting to build a barn or outbuilding first without having a principal structure which isn't allowed. They ask about the principal structure requirements. Our ordinance states 30'x40' (1,200 SF) footing and a 720 SF minimum livable space, so they're proposing to build a 4,800 SF barn with a 720 SF apartment and then also want to build a house in the future.

- Accessory Dwelling Units (mother-in-law/MIL apartments) – I'm seeing these more and more. They are subordinate, but one came to us in concept as a 1,200 SF house with 2-car garage basically another house and that is not the true intention of the ordinance, but the ordinance doesn't really speak to that.

Gardner – They are not allowed to rent that though. It has to be for family.

Kaltsas – When building a nicer or as nice ADU as their house. This one also is out in front.

Dumas – Are they allowed to AirBnB those?

Kaltsas – No ordinance currently prohibits that, and we don't have short-term rental ordinances.

Dumas – All these people who build the ADU for the mother-in-law and then what's the chance that the next person will use it that way.

Kaltsas – That's the problem. The one on Drake happened, Pagenkopf and Becker. The one on Drake is a rental and not doing what's intended. I'm ok but want it to be consistent and not skirting the rules.

Story – I see people taking advantage of that. When one has a mother/MIL coming to live with someone but then goes to sell it to a buddy who's gonna rent it out for the next 10 years.

Gardner- There is language about it needing to be a blood relative occupying it.

Kaltsas – For the ADU there is. It has to be a blood relative or relative owner, but the problem is enforcement.

Usset – Does it apply to other property?

Kaltsas – It does. When it sells, it is hard to enforce. Shouse / Barn houses size should there be any proportionality between residential and barn sizes? ADU's - when they are in the front yard and barely subordinate or are in nice neighborhoods, where it looks out of place, it is getting away from the intent. We might want to talk about these and get on the same page.

Dumas – What about the ADU on the lake?

Kaltsas – They're building the house, but now came back asking "What is attached?" They want to attach it on the other side of a breezeway. Our ordinance is silent on the definitions. Building code requires a common wall. Most are fairly legit. We'll need to discuss %, placement, setback, proximity, lot size, etc. Some want to build another driveway and have 2 homes/lots, but this skirts the rule.

6. Adjourn

Motion by Story, seconded by Usset to adjourn the meeting at 8:00pm.

Respectfully Submitted,
Linda Johnson/ Recording Secretary

MINUTES OF A MEETING OF THE
INDEPENDENCE PLANNING COMMISSION
TUESDAY, JANUARY 17, 2023, AT 7:30 PM

1. CALL TO ORDER

Pursuant to due call and notice thereof, a work session of the Independence Planning Commission was called to order by Chair Gardner at 7:30 p.m.

2. ROLL CALL:

PRESENT: Commissioners Gardner (Chair), Dumas, Volkenant, Thompson, and Usset.
ABSENT: Story and Tearse.
STAFF: City Administrator Kaltsas, Administrative Services Director Simon.
VISITORS: See Sign-In Sheet

3. Annual Election of Chair and Vice Chair

**Motion by Dumas to re-elect Gardner as Chair and Thompson as Vice Chair for another term.
Ayes: Gardner, Dumas, Volkenant, Thompson, and Usset. Nays: None. Absent: Story and Tearse. Abstain: None. Motion Approved. 5-0**

4. APPROVAL OF MINUTES:

- a. December 6, 2022, City Council Meeting Minutes (For Information Only)

Since there was no December meeting, the November minutes will be included for the February Planning Commission meeting.

5. Tom Koch (Applicant/Owner) is requesting that the City consider the following review/discussion for the property generally located at 5865 Koch's Crossing (PID No's. 11-118-24-12-0004, 11-118-24-13-0003, 11-118-24-31-0005, 11-118-24-13-0002, 11-118-24-42-0001, 11-118-24-42-0002) in Independence, MN:

- a. Final Plat to allow a 33-lot subdivision to be known as KOCH FARM SANCTUARY.

Request:

Tom Koch (Applicant/Owner) is requesting that the City consider the following review/discussion for the property generally located at 5865 Koch's Crossing (PID No's. 11-118-24-12-0004, 11-118-24-13-0003, 11-118-24-31-0005, 11-118-24-13-0002, 11-118-24-42-0001, 11-118-24-42-0002) in Independence, MN:

- a. Final Plat to allow a 33-lot subdivision to be known as KOCH FARM SANCTUARY.

Property/Site Information:

The overall property is comprised of six (6) individual properties located on the north and south sides of Koch's Crossing between County Road 90 and Independence Road. The property also touches Brei Kessel Road on the south. There is an existing home and a series of detached accessory buildings located across several of the properties. The properties are comprised primarily of agriculture land, a pond/wetlands.

Property Information: **5865 Koch's Crossing**

Zoning: Agriculture

Comprehensive Plan: Rural Residential

Acreage: ~141 acres

Discussion:

The City approved the rezoning, conditional use permit, variance and preliminary plat for the proposed development in 2022. The applicant has now submitted an application for final plat of the property for a 33-unit subdivision developed across the 6 subject properties. The City reviews the final plat to ensure consistency with the approved preliminary plat. There were several items that were noted during the review of the preliminary plat which have been addressed and revised by the applicant. The City provided guidance and feedback during the preliminary plat review process and has now completed a comprehensive and detailed review of the proposed final plat and associated construction drawings.

Proposed Cluster Subdivision

The applicant has prepared the final plat based on the cluster development standards. The applicant is proposing to meet all applicable cluster subdivision standards. The cluster subdivision standards are as follows:

Subd. 4. *Cluster development conditional use permit.* Cluster development is a conditional use in the Rural Residential District, subject to the provisions of subsections 520.09, 520.11 and 520.13 of this Code.

- (a) *Purpose.* The purpose of the cluster development conditional use permit is to promote the creative and efficient use of land. The provisions of this subdivision are intended to:
 - (1) Protect natural features in common open space.
 - (2) Improve the arrangement of structures, facilities and amenities on a site.
 - (3) Preserve the rural character of the community.
- (b) *Criteria.* A cluster development is a residential development in which a number of single-family dwelling units are grouped on smaller lots than in conventional developments, while the remainder of the tract is preserved as open space. If the following standards are complied with, density of one unit per four acres is permitted.
 - (1) The development parcel must be 40 or more acres in size;
 - (2) A minimum of 50% of the gross acreage of the subject property, excluding right of way dedicated for State, County and Existing City Roads, must be preserved as open space, recreational space or agricultural use;
 - (3) A minimum of 50% of the preserved open space, recreational space or agricultural use land must be useable. Wetlands, streams, lakes, ponds and lands within the 100 year flood plain elevation are not considered to be useable for the purpose of this subsection;
 - (4) Woodland, wetlands and topography must be preserved in a natural state, with modification allowed

when no reasonable alternative exists; or, if the site lacks unique features such as woodlands and wetlands, the site must be designed and constructed in such a manner that residential building sites are integrated into a created natural environment including reforestation, wetlands enhancement, and vegetative screening of structures;

- (5) The preliminary plat must show a primary and secondary individual sewage treatment site for each dwelling unit and must be supported with soil test reports indicating the adequacy of each proposed location; provided, that shared treatment systems within a development may be acceptable if the plat identifies two or more suitable sites for the shared system and the city council approves the proposal;
- (6) Lots within the development must have a minimum lot size of 1.5 contiguous buildable acres. Buildable acreage must not be separated by streams, wetlands, or other physical impediments;
- (7) Lots within the development must have a minimum of 150 feet of frontage on an improved public road or street, except lots fronting on the terminus of a cul-de-sac shall have no less than 50 feet of frontage.
- (8) Open space must be designated in the development as one or more outlots and must be owned either by a homeowners' association consisting of the owners of all of the residential lots in the development or by the owners of the residential lots, as tenants in common;
- (9) The developer must record against the development a declaration of covenants that places responsibility for management of the open space in a homeowners association and provides for the assessment of management costs to the association members and memorialized in an agreement with the City;
- (10) All utilities must be placed underground;
- (11) All residential streets within the cluster development must be paved with a bituminous surface according to the city street standards in effect at the time of the development;
- (12) A development agreement must be entered into with the city.

Lots Permitted Based on Cluster Subdivision

Total Area: 141.35 acres
138.75 net acres (subtracting CSAH 90 right of way)
138.75 x 50%: 69.40 acres of open space required
69.40 acres of open space provided
50% of 69.40 acres: 34.70 acres of useable open space required
40.60 acres of useable open space provided
33 Lots Proposed

Based on the cluster development standards, the applicant is proposing to develop the property in accordance with applicable provisions with several noted exceptions and or issues.

The City has completed a detailed review of the proposed subdivision as follows:

1. The proposed plan proposes to realign Koch's Crossing at the point of intersection with CSAH 90. The realignment of Koch's Crossing likely aids the sight lines at the CSAH 90/Koch's Crossing intersection. Hennepin County has approved the relocation of Koch's Crossing and associated turn lane plans. The County is requiring a northbound turn lane from CSAH 90 to Koch's Crossing. This turn lane combined with the relocated intersection should help to mitigate traffic impacts associated with

the proposed development.

2. Koch's Crossing will be fully upgraded to the City's improved street standards and will tie into the eastern half that was recently upgraded as a part of the Serenity Hills subdivision.
3. The City had asked the developer to provide for a future right of way connection to the west to accommodate a future road connection. The proposed final plat accommodates a future right of way that would allow a street connection to the west between Lot 1, Block 10 and Lot 3, Block 11. The applicant is not proposing to construct a stub street at this time.
4. The City requested that the applicant provide additional right of way around the perimeter bulb of the existing Brei Kessel cul-de-sac. The additional right of way would allow the bulb to be constructed to the City's standard at some point in the future. The applicant is dedicating an easement that will allow the expansion.
5. The City requested that the applicant provide a paved trail connection between the cul-de-sac bulb on Brei Kessel and the cul-de-sac bulb proposed on William Way. The applicant is proposing to construct a new bituminous trail connection between the existing and proposed developments.
6. The City noted the existence of an existing RIM (reinvest in Minnesota) easement. The applicant has removed the existing RIM easement from the private lots as recommended.
7. The applicant has provided information verifying each lot can accommodate a primary and secondary septic site.
8. The applicant shows all applicable building setbacks on each proposed lot along with a proposed house pad. Applicable setbacks are as follows:
 - Front Yard Setback: 85' from centerline of road
 - Corner Yard Setback: 52' from property line
 - Side Yard Setback: 30' from property line
 - Rear Yard Setback: 40' from property line
 - Wetland Setback: 10' from edge of wetland buffer
9. The applicant is proposing to preserve existing trees located within the proposed open space/Outlots.
10. The City requires that the applicant provide a planting and maintenance plan for the Outlot areas to be maintained by the HOA. The applicant has provided the City with a disturbed upland buffer planting and maintenance plan.
11. The applicant is proposing to place a conservation easement over and across Outlot D. This will provide an additional layer of protection and preservation of the pond and woodlands.
12. The proposed final plat is not proposed to be phased and all public improvements will be constructed at the same time. Developing the public improvements at one time should help mitigate the impacts of construction on the surrounding properties.
13. The existing portion of Koch's Crossing that is proposed to be eliminated will be formally

vacated at the same time as Council consideration of the final plat. The applicant has filed a petition to the City seeking vacation of a portion of Koch's Crossing. Vacation of the portion of Koch's Crossing will be subject to the dedication and construction of the new right of way and associated street.

14. The City's water resource engineers have completed a detailed review of the proposed plans noted in the review letter dated January 3, 2023, by Hakanson Anderson. The applicant will be required to revise the plans in accordance with all applicable comments.
15. Applicant will be required to secure all applicable approvals from Pioneer Sarah Creek Watershed.
16. It is noted that a Wetland Replacement plan will be required for the wetland impacts that are proposed. The Applicant shall submit a Wetland Replacement Plan for review and approval.
17. The City's engineering consultant has reviewed the plans and prepared a detailed review of the final plat as noted in the letter dated January 3, 2023, by Bolton & Menk. The applicant will be required to make all applicable revisions to the plans based on the comments provided.
18. The proposed subdivision is subject to the City's Park dedication requirements. The City is not requesting any public park land within the development. It is anticipated that the proposed useable open space would be fully accessible to the development and there appears to be good connectivity to all of the proposed Outlots. The standard park dedication requirement of \$3,500 per lot will be applicable to all newly developed lots. The City has agreed to credit the development for the six (6) existing parcels (this would reduce the total from 33 to 27 lots). The total park dedication for this property will be **\$94,500.00** (27 lots x \$3,500 per lot).

Recommendation:

The Planning Commission is being asked to consider the request for Final Plat with the following findings and conditions:

1. The proposed Final Plat meets all applicable conditions and restrictions stated Chapter V, Section 510, Zoning, in the City of Independence Zoning Ordinance.
2. City Council approval of the Final Plat will be subject to the following:
 - a. The Applicant shall make all revisions required and as noted within this report, by the Planning Commission and City Council.
 - b. The Applicant shall address all comments and applicable requirements pertaining to the water resources and engineering as outlined in the associated review letters from Hakanson Anderson Associates and Bolton & Menk, Inc.
 - c. The Applicant shall make all recommended and required revisions and receive final approval for all proposed and regulated improvements from Hennepin County.
 - d. The Applicant shall comply with all applicable regulations and conditions prescribed by Pioneer Sarah Creek Watershed Management Commission.

- e. The Applicant shall enter into a development agreement with the City for this development.
 - f. The Applicant shall provide a letter of credit as established by the development agreement for all public improvements associated with this development.
 - g. The Applicant shall provide the City with copies of the HOA agreement and covenants, including information related to the maintenance of the common driveway.
 - h. The Applicant shall obtain all necessary City, County, PCA and other regulatory agency approval and permits prior to construction.
 - i. The Applicant shall pay the park dedication fees in accordance with the terms defined in the Development Agreement. The Applicant shall consent to the establishment of the storm sewer improvement tax district.
 - j. The Applicant shall enter into a storm water maintenance agreement pertaining to the required storm water ponds to be located on the property.
 - k. The Applicant shall submit a Wetland Replacement Plan for review and approval.
 - l. The Applicant shall provide prospective buyers with the Letter of Information Pertaining to the subdivision as prepared by the City.
 - m. Koch's Crossing will need to be vacated by the City. Vacation of the right of way will need to correspond with the establishment and construction of the new right of way and road.
3. The Applicant shall pay for all costs associated with the City's review of the final plat.
 4. If the final plat is approved by the city council, the subdivider shall record it with the county recorder within 90 days after the date of approval; otherwise, the approval of the final plat shall be considered void.

Kaltsas- This was previously considered by the Planning Commission and City Council had approval of the preliminary plat along with a series of things- including rezoning, CUP/conditional use permit to allow a cluster development, etc. The applicant has taken into account comments and brought back a final plat application for the 33-lot cluster development to be known as Koch Farm Sanctuary. This property was rezoned from AG/Agriculture to RR/Rural Residential and its 141 acres of 6 different properties. The City approved a 33-lot cluster subdivision according to the City's cluster standards and made a couple changes to those standards. Final plats are reviewed by the City, but we typically look to see that it is consistent with preliminary plat and were conditions of it accommodated and/or exchanged by the applicant. And if it's largely staying consistent with it, then the City would typically approve it. We'd do a detailed technical review of stormwater, outside agency comments (watershed), Hennepin County's comments. With that we did a more detailed technical review of the development and are awaiting drainage and engineering comments. From the high-level comments that came from the discussion, the Applicant is proposing to realign Koch's Crossing from its current configuration and in that realignment will be vacating a portion of the existing Koch's Crossing and then constructing a

dedicated turn lane into it per Hennepin County's review and approval process. They're also proposing additional Co Rd 90 ROW/right of way to accommodate that turn lane and realign Koch's Crossing. It will then be a fully upgraded street section bituminous road similar to Serenity Hill. The City had asked the developer to provide for a future right of way connection to the west of the property. In the report you can see that they're providing between lots Lot 1, Block 10 and Lot 3, Block 11 a full 66 ft wide right of way to accommodate a future road connection to the west if and when that develops. The City requested that the applicant provide additional right of way of the perimeter bulb of the existing Brei Kessel cul-de-sac noting it was a substandard cul-de-sac that had been built as a temporary. While the City did not require it be connected or extended through, they did request additional right of way which is shown in the plan as an additional bulb width in that outlot for future drainage utility. With that connection not being required as a full street connection, the City did request that a full trail connection between the two as noted in the packet. It's a full 8 ft bituminous trail going end to end and goes across the creek essentially connecting end of cul-de-sac to end of cul-de-sac from a pedestrian standpoint. The additional right of way would allow the bulb to be constructed to the City's standard at some point in the future. The applicant is dedicating an easement that will allow the expansion. Outside of those comments the City looked at the conditions of the preliminary plat accommodations and feel the final plat is consistent and addressed by the applicant. So now it's being asked for final plat approval for the 33-lot development. We reviewed stormwater, grading, and drainage, and a couple items were noted in the comment letters: detailed "change this" which will get updated and revised. Council will adopt development agreement to assure it is built to standards. Historically we look at our larger lot developments at a Final Plat level in front of Planning Commission and again most heavy lift we do at Preliminary Plat. The final plat is to see the set of plans comply with conditions imposed during that Preliminary Plat.

Thompson – Point 4. The additional easement to make it a standard size cul-de-sac is addressed, but why isn't building that cul-de-sac a part of this agreement?

Kaltsas- The City discussed it but ultimately the City did not require the applicant to reconstruct Brei-Kessel cul-de-sac. It is just for future development and burden was not put on the applicant. We asked for the additional right of way so it could be built to full width. The burden was not imposed on the North side developer.

Thompson – Because it is a substandard cul-de-sac, correct?

Kaltsas – Correct. It's less of a radius end was built with a temporary easement not a full dedicated right of way. Part of that temporary easement burdens one of the private property owners but it is a legitimate easement in place. The other was to be sure the other side accommodates the future expansion.

Thompson – Point 5. With the quality or size of trail, is there any public safety usage or consideration for Emergency services. Is it restricted to only pedestrian and bike use only?

Kaltsas – It is for pedestrians/bikes only. Emergency services did not want access and we had specifically asked, and it was not asked for by them. This would be a standardized trail section with some details of grades still being worked on. It would be a standard public bicycle/pedestrian trail with a 8-8.5 ft width.

Thompson – What about winter maintenance?

Kaltsas – It’s undecided if we would plow through, but it would accommodate typical pickup truck with plow. I think people would use it.

Gardner - Have you talked to adjacent owner of the stub going West?

Kaltsas - We did in the beginning, and they were in favor of the stub going to the west. The location is somewhat arbitrary and we kind of pick the top of the hill for a typical design. That road may be a little difficult, but it provides for the best location to get access going to the West. If there were a development to come in, it was available to connect.

Volkenant – I wasn’t here at the time but is this a similar situation as to Brei-Kessel.

Kaltsas- 100% the same. No public hearing is involved at this point.

Motion by Thompson, seconded by Volkenant to recommend approving resolution for Tom Koch at 5865 Koch’s Crossing recommending approval of Final Plat for the development to be known as Koch Farm Sanctuary. Ayes: Gardner, Dumas, Volkenant, Thompson, and Usset. Nays: None. Absent: Story and Tearse. Abstain: None. Motion Approved. 5.0.

This goes on to the February 7th City Council Meeting.

6. PUBLIC HEARING: Chad Greenway (Applicant/Owner) requests that the City consider the following action for the properties located at 2171 Copeland Rd. and 2052 Nelson Rd., Independence, MN (PID No. 19-118-24-14-0001 and 19-118-24-13-0003):

- a. A minor subdivision to permit a lot line rearrangement to adjust the property line that runs east and west between the two subject properties.

Request:

Chad Greenway (Applicant/Owner) requests that the City consider the following action for the properties located at 2171 Copeland Rd. and 2052 Nelson Rd., Independence, MN (PID No. 19-118-24-14-0001 and 19-118-24-13-0003):

- a. A minor subdivision to permit a lot line rearrangement to adjust the property lines between the two subject properties.

Property/Site Information:

There are two subject properties being considered. The applicant owns both properties and has an existing home on the north property and recently located a detached accessory structure on the south property. The north property has frontage on Copeland Road and the south property has frontage on both Copeland Road and Nelson Road. The property has the following site characteristics:

Property Information: 2171 Copeland Road and 2052 Nelson Road
Zoning: Agriculture

Comprehensive Plan: *Agriculture*

Acreage (Before): *PID No. 19-118-24-14-0001*

2 acres

PID No. 19-118-24-13-0003

3.4 acres

Acreage (After):

5.4 acres

Discussion:

The applicant acquired a detached accessory structure (~22' x 35') and located on the south property which is defined as Parcel A (no principal structure). The applicant inquired whether or not a building permit was required for the accessory structure. The City reviewed the structure and noted that the structure would require a zoning permit and would have to be located on the same property as the principal structure (identified as Parcel B). The applicant asked if they could adjust the property line between their two properties. It was noted that lot line rearrangements are permitted in the AG-Agriculture zoning district. The applicant is now seeking consideration of a lot line rearrangement to expand the boundaries of Parcel B and reduce the boundaries of Parcel A.

The applicant is proposing to add 2.88 acres from Parcel A to Parcel B in the after condition. Parcel A will be reduced from 60.84 to 57.96 acres and Parcel B would be increased from 10.00 to 12.88 acres.

There are several items that should be noted by the City during consideration of the application:

1. The City does not have accessory structure square footage limitations for properties that are greater than 10 acres.
2. Parcel A in the after condition would still have a single building eligibility.
3. The proposed (existing) detached accessory building meets applicable building setbacks in the after condition.
4. The existing perimeter drainage and utility easements should be vacated and the requisite perimeter easements relating to the revised lot boundaries will need to be dedicated to the City. The applicant shall execute the necessary documents to convey the easements as determined necessary by the City.

The proposed subdivision to allow a lot line rearrangement appears to meet all applicable standards of the City's zoning and subdivision ordinance. The revised lots will continue to be consistent with the City's zoning ordinances and Comprehensive Plan.

Neighbor Comments:

The City has not received any written comments regarding the proposed minor subdivision.

Recommendation:

Staff is seeking a recommendation from the Planning Commission for the requested minor subdivision with the following findings:

1. The proposed subdivision allowing a lot line rearrangement meets all applicable criteria and conditions stated in Chapter V, Section 500, Planning and Land Use Regulations of

the City of Independence Zoning Ordinance.

2. The applicant shall dedicate the requisite drainage and utility easements to the City. The applicant shall execute the necessary documents to convey the easements as determined necessary by the City. It is recommended that the applicant request vacation of the existing perimeter drainage and utility easements.
3. The applicant shall pay for all costs associated with the City's review of the requested minor subdivision.
4. The applicant shall record the subdivision and City Council Resolution with Hennepin County within six (6) months of approval.
5. The applicant shall execute and record the requisite drainage and utility easements with Hennepin County within six (6) months of approval.

Kaltsas – This is an application for a lot line rearrangement to adjust property lines between the two subject properties at 2171 Copeland Road and 2052 Nelson Road. Applicant Chad Greenway approached the City about adding another structure on the property. It is a recently subdivided property of which he owns 2 of the 3 properties. They constructed an initial principal structure on the 2171 Copeland Rd property and there is no structure on the 2052 Nelson Rd property. So with that they won't be able to add any other accessory structures on that property without a principal structure. They'd like to utilize some of the 2052 property, and they can rearrange the property line between these 2 properties. Both are zoned as and guided by the City's CompPlan as AG/Agricultural properties. He is proposing taking 2.88 acres from parcel A and adding to Parcel B, thus reducing Parcel A from 60.84 to 57.96 acres and Parcel B would be increased from 10 acres to 12.88 acres. After surveys show Parcel A as the larger 60 acres and Parcel B (NE corner) would be increased by 2 parcels. The property's principal structure is a Barndominium but does count as it meets the City's sq footage requirements. Within the AG zoning district, a couple of things are allowed for subdivisions. A lot line rearrangement is one of the allowed subdivision provisions. So, making one parcel 2.88 acres larger and reducing the other by the same does not impact the remainder properties. The property boundaries would be expanded so that the existing principal structure would have larger setback at side and rear lot lines, but no nonconformities would be created, and the proposed shed now on Parcel B would meet applicable setbacks.

Gardner – Was the barn a primary residence?

Kaltsas – Yes, it's a Barndominium.

Thompson – Was the structure already there?

Kaltsas – Yes. It was brought in on a truck instead of being built and this is typical for this process.

Thompson – It doesn't change the nature of the development rights?

Kaltsas – The second property has no impact because there's still only one eligibility on that property because they utilized the eligibilities for these two 10 acres lots. There's still a singular eligibility on Parcel A. On Parcel B, there's still a single eligibility, it just expands it by that amount.

Gardner – And they could do this or move the building?

Kaltsas – Yes. They could put it in a different location, but there are some challenges on Parcel B. They did a full delineation and with some pockets of wetlands through there, this seemed to be where it would work for them.

Gardner – When was this subdivided?

Kaltsas – It was subdivided just a year and half ago.

Gardner opened the public hearing. Motioned by Thompson to close, seconded by Volkenant to close.

Gardner - It sounds simple to me. He's making it right, is all. (Agreement.)

Thompson – And the fact that it doesn't change eligibilities and opportunities for development in any ways. It's a no brainer.

Motion by Volkenant, seconded by Usset to recommend consideration for the lot line rearrangement for 2171 Copeland Road and 2052 Nelson Road for the minor subdivision and adjustment of property line that runs east and west between the two subject properties. Ayes: Gardner, Dumas, Volkenant, Thompson, and Usset. Nays: None. Absent: Story and Tearse. Abstain: None. Motion Approved. 5.0.

This goes on to the February 7th City Council Meeting.

7. PUBLIC HEARING: William Stoddard (Applicant) and John Zeglin (Owner) requests that the City consider the following actions for the property located at 9285 US Hwy 12, Independence, MN (PID No. 18-118-24-21-0001):

- a. A comprehensive plan amendment to allow a portion of the property to be re-guided to the Urban Commercial - UC zoning district. A portion of the property is already guided Urban Commercial.
- b. Rezoning of a portion of the property from AG-Agriculture to UC-Urban Commercial.
- c. Conditional Use Permit to allow a planned unit commercial development on the subject property. The proposed development would include office warehouse and private garage condominium buildings.
- d. Site Plan Review to allow the proposed development.
- e. Preliminary Plat to allow the subdivision of the property into eight (8) residential lots and associated lots to allow the proposed commercial development.

Request:

William Stoddard (Applicant) and J F Zeglin Jr & M D Zeglin (Owner) requests that the City consider the following actions for the property located at 9285 US Hwy 12, Independence, MN (PID No. 18-118-24-21-0001):

- a. A comprehensive plan amendment to allow a portion of the property to be re-guided to the Urban Commercial - UC zoning district. A portion of the property is already guided Urban Commercial.
- b. Rezoning of a portion of the property from AG-Agriculture to UC-Urban Commercial.
- c. Conditional Use Permit to allow a planned unit commercial development on the subject property. The proposed development would include office warehouse and private garage condominium buildings.
- d. Site Plan Review to allow the proposed development.
- e. Preliminary Plat to allow the subdivision of the property into eight (8) residential lots and associated lots to allow the proposed commercial development.

Property/Site Information:

The property is located on the south side of Highway 12 and west side of Nelson Rd. The property has frontage on both roads and is comprised primarily of agriculture land, woodlands, and wetlands. There is an existing home and several detached accessory structures on the subject property.

Property Information: 9285 Highway 12

Zoning: Agriculture

Comprehensive Plan: Agriculture/Urban Commercial

Discussion:

The City reviewed a concept site plan in 2022 for this property and provided feedback to the applicant and staff. The applicant considered the feedback and direction and has now made a formal application to develop the subject property. The applicant has revised the proposed development to include eight (8) single-family lots along Nelson Road, 120-130 lifestyle auto condominiums and 10,600 SF of commercial/retail space. The proposed lifestyle auto condominiums would be developed to allow for vehicle storage and would include amenities associated with this type of product (i.e., loft, bathroom, etc.). The applicant and City would work to establish regulations to prohibit overnight stays and limit and define any larger events (i.e., auto show) that could occur on the property. It is acknowledged that the proposed lifestyle auto condominiums would produce less traffic, require fewer public resources, and provide more market valuation than the initial commercial/warehouse development. The facility would be secured, professionally managed, and generally concealed from Highway 12 and Nelson Road.

Approximately 10,600 SF of commercial retail is proposed by the applicant. This area would have Highway 12 frontage and visibility consistent with the City's Comprehensive Plan. The City has reviewed the initial site plan layout and is generally in agreement with the proposed layout. The City would require a more detailed site plan specifically for the proposed Lot 9 development. The plan will need to provide more detail relating to allowed and proposed uses, parking and drive aisle dimensions, landscaping and lighting. In addition, the City will want to consider how/if the site could or would accommodate retail sales such as a drive thru and restaurant. A more detailed parking analysis would need to be completed.

In order for the City to consider approval of the proposed plan, the following approvals and steps would be required:

1. Amend the Comprehensive Plan.
 - a. This would re-guide a portion of the property (~16 acres) from AG-Agriculture to Urban Commercial.
2. Rezone that portion of the property indicated from AG-Agriculture to Urban Commercial.
3. Consider Site Plan Review.
4. Consider a Conditional Use Permit to allow a planned unit commercial development on the subject property.
5. Consider Preliminary Plat approval.
6. Consider Final Plat approval.

The following land uses are proposed by the applicant (plans attached):

- Commercial office/retail (10,600 SF on ~ 1.87 acres)
- Lifestyle auto condominiums (102 units on ~ 11.65 acres)

- Eight (8) Residential Lots (approximately 4.5 acres each)
Acreage: ~58 acres

There are several key points of information that should be noted relating to the proposed development:

- The entire property is currently zoned AG-Agriculture.
- A portion of the property, approximately 12 acres on the west side is guided by the City's Comprehensive Land Use Plan for Urban Commercial.
- The applicant is asking the City to consider rezoning a larger portion of the property from AG-Agriculture to Urban Commercial (~16 acres).
- The remainder of the property (~28 acres) would remain as AG-Agriculture.
- In order for the property to be rezoned, the City would first need to approve a Comprehensive Plan Amendment. A comprehensive plan amendment process would be subject to approval by the Metropolitan Council.
- The initial submittal considered access to Nelson Road and the applicant was notified that the City would not support any commercial access to Nelson Road. The plans submitted include a right in/right out only onto Highway 12 with a future frontage road connection to the west that could connect to County Line Road (shown on site plan). MNDOT has reviewed the plan and provided formal comments to the City and applicant. MNDOT has commented that they would recommend access to this site be provided via a connection to Nelson Road or County Line Road rather than a right in/right out as proposed. As a result of the MNDOT review, access has not been finalized. The issue of access will need to be resolved prior to the City finalizing a decision relating to this application.
- The applicant has provided the City with a traffic study as requested. The study indicates that the proposed development would generate a total of 7 entering and 1 exiting trips during the morning traffic peak and 2 entering and 7 exiting during the afternoon traffic peak hour. A total of 68 trips per day would be generated by the proposed auto condos and commercial retail uses. This would equate to approximately 1 trip every 7 minutes during peak times and approximately 2 trips per hour throughout the remainder of the day (see table below). It is noted that the proposed right in/right out would allow access to the proposed development until such time as a frontage road could be constructed to connect to County Line Road. Staff is seeking direction relating to access and the overall proposal from the Planning Commission and City Council.
- Following discussion with Planning and Council, the applicant has revised the plans to include eight (8) residential lots along Nelson Rd. The proposed residential lots mirror the lots across Nelson Rd. to the east. Development of the west side of Nelson into residential lots that are similar to those in existence would provide additional buffering of the proposed commercial development. It is noted that the existing residential lots directly across and on the east side of Nelson Road from the subject property have approximately 200 LF of frontage. The City's current ordinance would require 250 LF of frontage for all lots greater than 3.49 acres. This would include Lots 1, 2 & 6-8. Lots 3-5 comply with the frontage requirements. The applicant could revise the plan to reduce the acreage of the proposed lots or increase the frontages to meet the specified requirements. The applicant is attempting to mirror the lots across the street; however, those lots are all less than 3.49 acres in size.
- The City noted in its initial review that the potential wetland impact associated with the development of driveways to serve the lots was a concern. The applicant has revised the plans to show the extent of the wetland impacts and is now proposing to utilize shared driveways for Lots 1&2 and 7 &8. Utilization of shared driveways would reduce the wetland impacts and the number of driveways located along Nelson Road.

- The applicant is proposing to provide on-site sewer (septic) and on-site water to serve the proposed development. The applicant has provided a report indicating how the development of on-site water would serve the fire suppression requirements for the proposed development. The City is continuing to evaluate and study the possibility of establishing a municipal well/water service in the location of the Urban Commercial to serve commercial development on the north and south sides of Highway 12. Should the proposed development move forward, the City will provide additional information relating to a municipal well to Council for consideration.
- The applicant has completed a wetland delineation for the property. The City initially provided feedback to the applicant relating to the potential wetland impacts. The applicant is proposing to mitigate the impacted wetlands utilizing the purchase of wetland credits. Approximately 31.36 acres of the subject property is wetlands. The applicant is proposing to impact 2.48 acres of the total wetland area (> 8%). The potential wetland impacts would need to be reviewed and approved by the City. Further review of the wetland mitigation plan would occur following an initial City approval.
- Stormwater management has been proposed for the development. The City has completed an initial review of the stormwater management plan and provided high level comments to the applicant. The proposal includes 3 stormwater ponds that would accommodate stormwater generated from the commercial portion of the proposed development. The City will continue to work with the applicant and would anticipate a more detailed submittal and calculations relating to the stormwater plan should the project proceed to final plat.
- The applicant is proposing to preserve a large portion of the mature trees on the property (see gray area below).
- A preliminary plat will need to be submitted.

The applicant has provided the City with a robust package of details and information relating to the proposed development. The proposed lifestyle auto condominium illustrations appear to utilize high quality building materials and a sophisticated design that is unique for this product in the metro area. There are many details that will need to be finalized prior to the City being able to formally act on the proposed site development and preliminary plat. Due to the significant number of pieces included in the proposed site development, staff would like additional direction and feedback relating to the overall proposal so that additional review and direction can be provided to the applicant. In particular, staff is seeking additional review and direction relating to the proposed land use, expansion of the UC – Urban Commercial zoning district, access to the property and the development of the residential properties on Nelson Road. It is anticipated that following discussion and additional direction, the City would be able to work with the applicant to finalize the preliminary plans and bring back additional information and any revisions recommended for further consideration. It is also anticipated that the City and applicant will have further discussions with MNDOT relating to their recommended access for this property.

Recommendation:

The applicant is seeking feedback from the Planning Commission pertaining to the proposed development.

Kaltsas – This had previously come before the Planning Commission and City Council as a concept plan but has changed somewhat. The Applicant is back with full submittal seeking several items: A comprehensive plan amendment to allow a portion of the property to be re-guided to the Urban Commercial from Agriculture. A portion of the property is already guided Urban Commercial. The rezoning of that property

from AG-Agriculture to UC-Urban Commercial if that CompPlan amendment were to be approved. A Conditional Use Permit/CUP to allow a planned unit commercial development. The proposed development would include retail office warehouse building and the private condominium garage buildings. A Site Plan Review to allow the proposed development. And a Preliminary Plat to allow the subdivision of the property into (8) residential lots associated with the proposed commercial development.

The property is located at 9285 Hwy 12 guided by the City's CompPlan as both AG and Urban Commercial and currently zoned Agriculture. It's approximately 58 acres in overall size. Previously, the Applicant's Concept Plan proposed a proportion of the property for an office warehouse about 300,000 sqft along with 3 residential lots in the SE corner of the parcel. The City provides general feedback without decision or determination. He has a revised plan seeking 8 single family lots along Nelson Rd, 120-130 lifestyle auto condominiums, and 10,600 SF commercial retail space on the north side of the property closest to Hwy 12. The proposed lifestyle auto condos would allow for vehicle storage with amenities of a loft, bathroom, and some upgraded facilities. They are individually owned so sold and owned by individuals. The Applicant noted some general regulations usually talked about including prohibition of overnight stays and limitations of larger events like auto shows which the City would want to further discuss if this were to move forward. Part of the 300,000 sqft office warehouse requirement was supposed to have a controlled intersection either at County Line Rd or if at Nelson Rd. needing a broader 2-way in and out full access intersection. Without it, that product is not capable of sustaining itself. It's not able to have just a limited access right in right out given the configuration with Hwy 12 and the inability to turn around on both sides like a roundabout or something. The Applicant came back with a product that does work with just a right in right out limited access off Hwy 12 and also carries a lot less daily traffic demand. That is these lifestyle auto condominiums. These Condos are everywhere right now across the country and are in Medina and another going in Watertown. They are providing a market study showing there is demand for this product. Proposing 10,600 sqft commercial retail on the Hwy 12 frontage side where it is guided Urban Commercial with "For Lease" space with Hwy 12 visibility. Additionally proposed are (8) single family lots on Nelson Road. Some discussions previously were screening from Nelson Rd and existing residents from Nelson Rd, and the 3 residential lots on the SE corner of the site. They discussed mirroring the residential lots that would mirror the lots on the East side to the West side of Nelson Rd where the property lines align. Approximately, 10,600 sf on 1.87 acres Lot 9 (multi-tenant commercial retail space with parking). Lifestyle condominiums 120-130 depending on the width would occupy about 11.65 acres. And (8) residential lots approximately 4.5 acres each. Noting the entire property is zoned AG/Agriculture. About 12 acres (shown in red) on the West are zoned as Urban Commercial in the City's CompPlan. The acres are divided as 12 acres (shown in red) and approximately 16 acres (shown in purple, middle) which the Applicant is seeking to be rezoned as Urban Commercial/UC, and the other (green) remains as Agriculture. From the City's CompPlan pictured, the blue dashed line shows the total property outline. About 12 acres is guided as Urban Commercial/UC and the remainder as long-term as Agriculture. This CompPlan was adopted in 2008-2009, and it was carried over in the latest CompPlan for both 2030 and 2040 maintaining an Urban Commercial node at the corner of Hwy 12 and County Line. Initially, the City Council said they would not support Nelson Rd as an access point with limited access with MNDot (right in, right out) if that section of the highway were upgraded in the future. We discussed with the Applicant about obtaining an easement allowing access across the adjacent property to the west that could be taken out to County Line Rd and utilize a controlled intersection at County Line Rd. The applicant was unable to obtain an easement from the adjacent owner, but the Applicant claims they have an access on Hwy 12 to serve this. MNDot would rather see a connection to Nelson Rd or County Line Rd without a right in right out/RIRO but we do not have resolution to that issue. The Applicant is proposing (& we are looking for feedback) a right in right out/RIRO access to be something temporary and will eventually connect to County Line Rd. We'd anticipated that a portion of this land guided for Urban Commercial/UC would have access to County Line Rd. There is consideration to see if we need to obtain an easement to allow a future frontage road from Nelson Rd all the way to County Line Rd to have a more controlled access to Hwy 12. With that said, the Commercial portion, there would

have to be some sort of temporary access right in right out/RIRO if we are not allowing Nelson Rd access without going into eminent domain if the City were to do that. The traffic study analysis for the proposed plan shows 7 entering and 1 exiting trips during morning peak and 2 entering and 7 exiting trips at afternoon peak about 68 daily trips. Approximately 1 trip every 7 minutes during peak times and 2 trips per hour during the remainder of the day. The proposed RIRO would be a temporary and secondary condition at some future point. Just for context if there were questions, I noted that there was no mention of the traffic impact to the 8 residential units. Typical trip generation ITE manual would state a typical residential property generates .75 peak AM and 1 peak PM, so 6 AM and 8 PM trips from those Nelson Rd lots.

Noting the 8 single family proposed lots could help the City with ensured land use or screening of the future commercial land use. That way residents who purchase the lots would have notification like current residents get when commercial property potentially adjacent and be aware of going into it. The existing residential lots directly across have 200 lineal ft each. The City's current ordinance would require 250 LF of frontage for all lots greater than 3.49 acres. This would include Lots 1, 2 & 6-8. Lots 3-5 comply with the frontage requirements. The applicant could revise the plan to reduce the acreage of the proposed lots or increase the frontages to meet the specified requirements. The applicant is attempting to mirror the lots across the street; however, those lots are all less than 3.49 acres in size. Those lots could be reduced if taking some of the depth off. The City noted in its initial review that the potential wetland impact associated with the development of driveways to serve the 8 lots was a concern. The applicant has revised the plans to show the extent of the wetland impacts and is now proposing to utilize a couple of shared driveways for Lots 1 & 2 and 7 & 8. (Red shows the impact of the wetlands.) Utilization of shared driveways would reduce the wetland impacts and the number of driveways located along Nelson Road. A shared driveway to serve Lot 6 through the wetlands and another through Lots 7 & 8 is a change and has been adjusted since the initial submittal. Impact to wetlands has to be approved by a panel of people including the City, authority over wetlands, showing you did everything possible to avoid potential wetland impact. Those driveways were just one area, and we showed other areas where they could be further reduced. The Applicant looked at the rest of the plan, and the Auto Condominiums were also adjusted for wetland. Ultimately, if it were to get approved, there would be wetland mitigation and replacement required. They are proposing mitigating wetlands offsite by the purchase of or acquisition of credits which is fairly common when using larger scale development like this. The wetland mitigation and plan would come after a City approval which would come at a Preliminary Plat level and at a future review by the City and other agencies. It would be served by onsite septic sewer, onsite septic, and has an elaborate plan. The City has looked at it from an initial level and ultimately review it with how it would be served showing septic fields able to accommodate the Auto Condominiums and also commercial retail, and similarly they'd provide the water service/wells to provide as water.

The City has no utilities to this site which was identified in the CompPlan when it was guided as Urban Commercial. We did look at onsite services being how we would serve this property noting that MetCouncil does not have facilities to serve it. Delano does but is unwilling to extend these services to the City, so unless Independence would need to allow Delano to annex the property on that border. So, short of that, an onsite system is a way to do that. The City did look at if the properties were to move forward to obtain permission by PC and CC, the City did a high-level study of providing for a municipal well to provide water services which is feasible should it move forward for this side of the road or the North side of Hwy 12. To provide suppression services for Fire, private wells is not the best. Should the proposed development move forward, the City will provide additional information relating to a municipal well to Council for consideration.

The Wetlands (green) impacts (red) – There is approximately 31 acres of wetlands and of the 58, he is proposing this would impact about 2.5 acres of wetland (less>8%). Stormwater management was provided with adding ponds by commercial retail (north), middle, and the SW corner. Preservation of a portion of the trees on the site around existing farmhouse and tree stand on the S side to the East. (grey area remaining) Tree canopy would remain in the after condition. There's high level information and high-quality

architectural products included. Tip up panel or post frame construction building. This has more substance compared to what we've seen. We must decide if this works well for what's guided for the City's Urban Commercial and if we want to expand to the original 12 acres. Secondary, the residents to Nelson Rd are not in our current system and ultimately provide to the 8 units. This property supports 1 Single family, and do we want to support 8 more? It could be closed off for development for some point in the future. This property has been on the market for 10-15 years, and we've had a wide array of different land uses on this property. We had people ask to guide entire area to something else or to a full residential area. 2-3 proposals of churches, all commercial. This is the first formal application to come forward. Recently, we have had a lot of activity on commercial development on the North side of Hwy 12 as well as this side for proposals. We did have more conversations with Delano for utilities, and Delano is not interested in serving it unless it is their property.

We do have a public hearing and thought we need to resolve the access piece before moving it to the City Council.

Dumas – re: RIRO/Right in right out- What is the proposed traffic control structure? Paint, curbs? In looking at the traffic report if that 10,600 sf ends up the Caribou Coffee, that would increase traffic.

Kaltsas – It is commercial retail supporting highway retail, office warehouse. A restaurant/ coffee shop throws off traffic control significantly. An insurance agency does not. A countertop warehouse does not. We would have to say what would be permitted.

Dumas – How far do you have to go to make the turn around?

Kaltsas – You would have to do the roundabout at 92, so go 1 mile. We just went through this for the updating Hwy 12. Roundabouts at County Line and 92 would provide for the westbound movement off Nelson Rd, and then a roundabout at County Line for the eastbound traffic. That is the spacing.

Dumas – Is there a barrier?

Kaltsas – You can't go left. We would have to figure that out. There isn't enough detail if MNDot would allow this.

Gardner - This is temporary until it spurs the interest of the space in between.

Thompson – With the parcel size and density feedback from MNDot did staff have a discussion about the proposal to future proof with an easement towards Nelson either shrinking lots 1-8 frontage-wise and having it be lots 1-7 and having an easement along the frontage?

Kaltsas – It might be the opportunity to obtain the ROW for the easement for future frontage road. Then that solves this development and the Nelson Rd issue of having a RIRO. In some of MNDot concepts for Hwy 12, there was discussion whether MNDot would obtain a frontage road, but it didn't make it all the way through their final process because of the cost of acquisition of land and cost of construction. If the City were to work with MNDot on that, it might allow for it to be constructed at some point.

Gardner – How are we guaranteed what kind of materials would be stipulated for the front of this?

Kaltsas – We would stipulate the architecture of that and garages' building materials. We don't have a standard for UC. It will come in under a PUD. Standards are when the development comes in and we can write the rules for that.

Gardner – Why are there 72 different septic systems. How many of those septic systems are serving how many units? Instead of putting in a big mound?

Kaltsas – We are not seeing huge sewer demand for auto condos. There is rough in for shower/bath. The usage is low. They are not lived in; they are recreational only. Full grading and fencing for septic areas. There would have to be more detail.

Thompson – There are frontage issues with residential lots and zoning, but it seems like an elegant solution to Nelson Rd as a residential community backing up to this. It is not a surprise, well screened, rather than pushing the commercial development right up to Nelson Rd. This seems like a good solution. What is the feedback creating 8 lots where there aren't 8 lots and the mirroring frontage?

Kaltsas – We've had lots of feedback and are concerned about traffic, or any development occurring and fully understand it.

Usset – How did the Nelson Rd current homes come to be there in the AG district?

Kaltsas – I don't have full history. Marvin might though. There was a different standard back then.

Gardner – They were split off before we had MetCouncil.

Dumas – The lots look deep. That's a good buffer.

Kaltsas – Existing is about 500 and the proposed lots are about 900. The Nelson Rd lots are shallower in depth than the proposed lots. This is a Plan Unit Development planning on screening the commercial.

Gardner – This buffer zone is unusual dimensions. Will the MetCouncil approve it?

Kaltsas – They don't have the ability to deny that. We have some grey area that allow for Rural Residential lots. It would be some sort of variance or exception to our rule. Tonight, it is difficult to do the next step. We want feedback - direction on the traffic piece and to hold a Public Hearing to get feedback as it's been postponed a few times.

Public Hearing was opened.

Bill Stoddard, Developer – The overall concept has changed with feedback from staff, and we've had some meetings already. On Nelson Rd, we went to mirror the lot widths across the street and made them deeper for more buffer instead of outlots, etc. Re: Septic and utilities – Our septic designer added primary and alternate sites are shown. We have a few meetings with MNDot with some Hwy 12 legal access. There are two access points on Hwy 12 already that we would be closing down. We're assuming that if we get approval on this concept, that we and the City would move that access to the East to avoid the wetland impact. Type 1, not type 3 (cattails). We are proposing a PUD commercial site so there would be a lot of HOA/homeowner association documents for no overnight stays, etc. My Chanhassen friends' unit is low volume. We're looking for a low impact commercial use, not a Caribou. Restrictive convenance on individual lots. We will be addressing animals, lot setbacks, driveways needing to be where they are approved, future commercial. It is all on the title. We want to compliment Nelson Rd.

Gardner – You talked with the neighbor on County Line Rd in between.

Stoddard – I've reached out but haven't had discussion yet. We could have future go that direction if we get to that point.

Thompson – What are your thoughts on MNDot feedback and discussion on future proofing easement with frontage road? Across Outlot C it wouldn't be a big deal, but it would impact the residential strategy for Nelson Rd from tip to tip.

Stoddard – I would like to get more engineering feedback. A frontage road isn't just on the North side. On the North side. from that since MNDot wants hundreds of feet on Nelson. Will it ruin lots 1 and 2? I'm not against it in theory, but that would have some future commercial traffic.

Thompson – Is there room to future proof these 30 years from now? To have a fully divided highway might be in everyone's interest.

Stoddard – I will talk with engineers more about feedback.

Ruth Clark – 2365 Nelson Rd – Who benefits from this other than the landowner? What other residents of Independence in the CompPlan. None of the residential lots are 4.5 acres. We are setting precedent here from Delano to Maple Plain along Hwy 12. What is the vision that the City has long term for commercial infiltrating AG properties? Does this fit that vision or is it capitulating for one owner?

Rick Stromer – 2828 County Line Rd / Maria Rd – I'm curious of the lighting of the units. Is it going to be fully enclosed with a fence? They are going to keep the trees. If I were to buy one of the new lots, I would cut down the trees right away. Can they control if people can cut down the trees when they buy the lots? We had to go through variance procedures to split out lot and we had to go through it why can't we? Since I back up to it, can I make 3 lots too? It looks like a nice place for a park as well. The garage thing is a nice thing, but the lighting is a concern. I know we do need to develop.

Gardner – It's a tough call. We do have lighting restrictions with lumens. It's a simple code issue. We could dictate some guidelines with saving trees in an area. Thank you.

Kathy Pluth – 2223 Nelson Rd – I moved out here for country 35 years ago. I love the open field when I turn on Nelson Rd. Putting those houses behind there, when my grandson comes down on Nelson, when the tar ends, he knows it's Grandma's. It will affect my property value. Traffic concerns and if we don't stand up with our ordinances and changing to commercial. I think we need to stand up for open spaces.

Deb Peterson – 2810 Nelson Rd – I drove into St. Louis Pk for 25 years turning right on Hwy 12 at 6:30am. You take your life into your hands every day. My concern is traffic, of course. There is enough traffic. There is property for sale back on Nelson Rd that would add to dirt road housing. People use Nelson Rd for a cut through Hwy 12 – Co Rd 6. It is going to be worse than 7 cars for entering / exiting.

Dave Potter – 2895 Nelson Rd. – I have been here for 22 years and have the largest boundary and closest to Nelson Road. I have enjoyed this view for 22 years. All good things must come to an end. I hope there would be no financial commitment burden to current Nelson Rd residents. And with an auto condo, are they really going to a roundabout when they leave or take a right? They might decide to take Nelson Rd going 140 mph. The plan looks great. We know this is going to happen. It can be policed and monitored. I appreciate the builder considering the residents and we want it to work for all. I like the buffer zone.

Mark Patterson – 2635 Nelson Rd – I graduated from Delano, moved back in 1992. I've been rear ended on that road. The traffic light by Coburns really helped. It is a temporary stop to get out onto Hwy 12. I don't know if a roundabout would help. There should be no entrance at all there. People have been talking about this spot for years. Why do you need a bathroom in every unit if it's a garage- car condos? I'm not sold on it and it could be a cheap home for someone. I'd rather have a Menards. Re: the First Lot: What is the storage building? (Existing building where the tractors are as a note.)

Stoddard – We are proposing to take down all the outbuildings other than the storage poll barn building? Mid 80s and some people think it brings value. It is there already so it has to be drawn in. (There's nothing sinister about it.)

Andrew Sonnenberg – 2455 Nelson Rd - Thank you for letting us all share our thoughts. My wife and I moved in and started having kids. This concerns me because each morning I take a left off Nelson – “life and death frogger.” There are high performance vehicles. One day my kids will be driving. Also, lighting is a concern. There are cattails along most of the stretch. Lastly, there is a federally designated wetland just down the road, and the flow downstream may be affected. Spring runoff water goes South through my backyard which is just North of the gravel turning to asphalt. I'm on the asphalt. I hear RPMS rev up from gravel on to asphalt. We were warned of how many dogs have been killed on the road. It used to be a farm road.

Gardner – You could use the service drive. For the residents to get to the stoplight would be a great thing instead of turning left.

John Zeglin - 2858 Nelson Rd. and this property's owner– The property is owned by my family. My driveway is 150 yards of the SE corner of the property. We have had a lot of people approach us on this property and everything has been shot down for a variety of reasons. It is frustrating as a property owner living on a Hwy for future development. We are 1/3 of a mile from a great area (Coburns, McDonald's, etc.) but are on the wrong side of the highway. I understand ordinances are needed. The City of Independence needs some development. We have fairly high taxes. Some developments would help. Mills Fleet Farm, Menards, and Target and all were “No's.” It is easy to be opposed to everything. I have to go to Nelson and

Hwy. 12 every day too. My family will benefit from selling the property. Coming out of my driveway, I have never turned left on the gravel road. No one with their nice cars will come down gravel. Cattails are on the property with lots of wetlands and there are rules being followed for its development. We have frustration with beautiful property, and our hands are almost tied on what we can do with the property.

Motion to close Dumas, seconded by Usset at 9:25pm.

Gardner – Are we going to continue this with a Public Hearing again?

Kaltsas – You closed it. If there is direction from the Planning Commission. I would recommend tabling it to the next meeting to see if there is some kind of resolution that we can bring back. I'd also ask that the Planning Commission give some sort of direction with residential piece and the expansion of commercial, what does that look like - # of units, is the area right. Thirdly, the small little Commercial/ Light Industrial. We've had discussions with the Applicant Obviously the initial proposal, which was for that larger light manufacturing warehouse, and because it needs access, it does not work. This smaller piece might allow for some. Long range the City's plan that was adopted has specifically restricted commercial development (1:44:14) except in 2 nodes at County Line and Co Rd 90. There's been focused areas that the City said and we get calls on it all the time because it's Hwy 12 frontage in Hennepin County. There is no interest that the City has had with further development of the corridor along Hwy 12. There is interest for the 30 acres on the north side and south side on County Line and around Co Rd 90. The CompPlan is specific on the commercial node. This property keeps coming up because it's broken by a line that the City drew where the 12 acres and 40 acres. They're selling 58 acres but what does the owner do since it's how the acres are divided? We get the same question on the north side too since there's high value highway frontage on the other side too.

Thompson – At some point there has to be a transition with Urban Commercial and the rest of the City, and because that UC line is within one property gives us an opportunity to use Eastern half with people knowing what they are moving into with the same Nelson Rd characteristics. It still attaches to what was already in the CompPlan. This is a good use of the parcel. The traffic concerns -not so much Nelson – MNDot will be heavy in that decision. We will be guided by what they do. Having a stub to West is nonnegotiable but having a stub to the East for a future proof as a potential easement. Maybe try to grab it now?

Usset – Auto Condos don't serve Independence residents but anyone can use them. Are auto condos the best usage of this buffer? I like the buffer concept. Why not put in auto condos where it is currently zoned and allowed, and see how it goes?

Thompson – It is not my position to say what kind of business as long it conforms to the use. What it does for the City? It is development, tax base, growth - even if 10 % is used by our residents. As long as the use conforms to the way we guide it and the ordinances, they're more than welcome to open a failed business. I'm not in the job of business planning. From investment, it sure would be full of pitfalls if we only allow them to start with a sliver of land first.

Dumas- Auto condos are probably a low use traffic spending all day shining up their cars up. Maybe the commercial piece should be Light Commercial- No Retail. As far as the Residential – maintaining a buffer and the character of the neighborhood is a reasonable approach.

Gardner – I don't think we can do much better than this, and we haven't seen a plan that is this palatable in 15 years. It isn't perfect. Menards, Target are not coming. They couldn't get sewer and prevented others from moving in it. We can wait 20 years for another option, but Mr. Zeglin would not want that.

Usset – What can we say to limit this from going further East?

Kaltsas – There would be houses in the way now. The Council and the City will have to be purposeful in the Comp Planning saying this is the end of the commercial node. The City has controlled the Hwy 12 frontage for a long time. We have had very little commercial development. Part of the concession of putting the residential in on Nelson is that you know you have a cut-off point. Could you jump on the backside on

those? Yes, you could. The control of that commercial node is high demand, and that property would have been developed a long time ago if it had utilities or let Delano annex it.

Gardner- Is there anything else that we could do to make it better?

Thompson – We should try to do municipal water and stay focused on that which is proposed.

Gardner – Is that a 26,000-gallon tank?

Kaltsas – The system would be high level, a municipal well system and not be a treatment plant. Like a lot of rural well systems.

Gardner – Will it be towered?

Kaltsas –It'll be towered or underground. A next phase study and feasibility report shows it can be done.

The cost would be borne by commercial or assessed to those properties.

Gardner – Are we clear on issues you'll be hammering out with the developer?

Kaltsas - There is good direction. I will go back and talk to the developer and MNDot. There is an obvious transportation issue. We started with No Access to Nelson as we don't want more traffic. The preferred connection is to County Line Road not to Hwy 12. But short of the City using eminent domain, which is not what the Council will likely do and we are not interested in doing that to our property owners, so it is right in right out/RIRO. If MNDot doesn't allow for it, then that is where we are.

Motion by Thompson, second by Dumas to table this item for commercial development along Hwy 12 for further consideration to the February 21st Council Meeting. Ayes: Gardner, Dumas, Volkenant, Thompson, and Usset. Nays: None. Absent: Story and Tearse. Abstain: None. Motion Approved. 5-0

8. Open/Misc.

Kaltsas – This will be on the next meeting along with some ordinance things for Accessory Dwelling Unit definitions.

9. Adjourn.

Motion by Usset, second by Thompson to adjourn the meeting at 9:38 pm.

Meeting adjourned.

Respectfully Submitted,
Linda Johnson/ Recording Secretary

MINUTES OF A SPECIAL MEETING OF
THE INDEPENDENCE CITY COUNCIL
FRIDAY, JANUARY 6, 2023–8:00 A.M.
City Hall Chambers

1. CALL TO ORDER.

Pursuant to due call and notice thereof, a special meeting of the Independence City Council was called to order by Mayor Johnson at 8:00 a.m.

2. ROLL CALL

PRESENT: Mayor Johnson and Councilors Betts and Spencer.

ABSENT: McCoy and Grotting.

STAFF: City Administrator Kaltsas and Assistant Administrator Simon.

VISITORS: None.

3. Approval of Accounts Payable (Checks Numbered 21708-21725).

Motion by Spencer, seconded by Betts to approve the Accounts Payables. Ayes: Johnson, Betts, and Spencer. Nays: None. Absent: McCoy and Grotting. Abstain. None. MOTION DECLARED CARRIED. 3.0

4. Adjourn.

Meeting adjourned at 8:03 a.m.

Respectfully Submitted,
Linda Johnson / Recording Secretary

■City of Independence

Review of a Proposed Business Park on the
Property Located at 9285 Highway 12

<i>To:</i>	Planning Commission
<i>From:</i>	Mark Kaltsas, City Planner
<i>Meeting Date:</i>	February 21, 2023
<i>Applicant:</i>	William Stoddard
<i>Owner:</i>	J F Zeglin Jr & M D Zeglin
<i>Location:</i>	9285 Highway 12

Request:

William Stoddard (Applicant) and J F Zeglin Jr & M D Zeglin (Owner) requests that the City consider the following actions for the property located at 9285 US Hwy 12, Independence, MN (PID No. 18-118-24-21-0001):

- a. A comprehensive plan amendment to allow a portion of the property to be re-guided to the Urban Commercial - UC zoning district. A portion of the property is already guided Urban Commercial.
- b. Rezoning of a portion of the property from AG-Agriculture to UC-Urban Commercial.
- c. Conditional Use Permit to allow a planned unit commercial development on the subject property. The proposed development would include office warehouse and private garage condominium buildings.
- d. Site Plan Review to allow the proposed development.
- e. Preliminary Plat to allow the subdivision of the property into eight (8) residential lots and associated lots to allow the proposed commercial development.

Property/Site Information:

The property is located on the south side of Highway 12 and west side of Nelson Rd. The property has frontage on both roads and is comprised primarily of agriculture land, woodlands

and wetlands. There is an existing home and several detached accessory structures on the subject property.

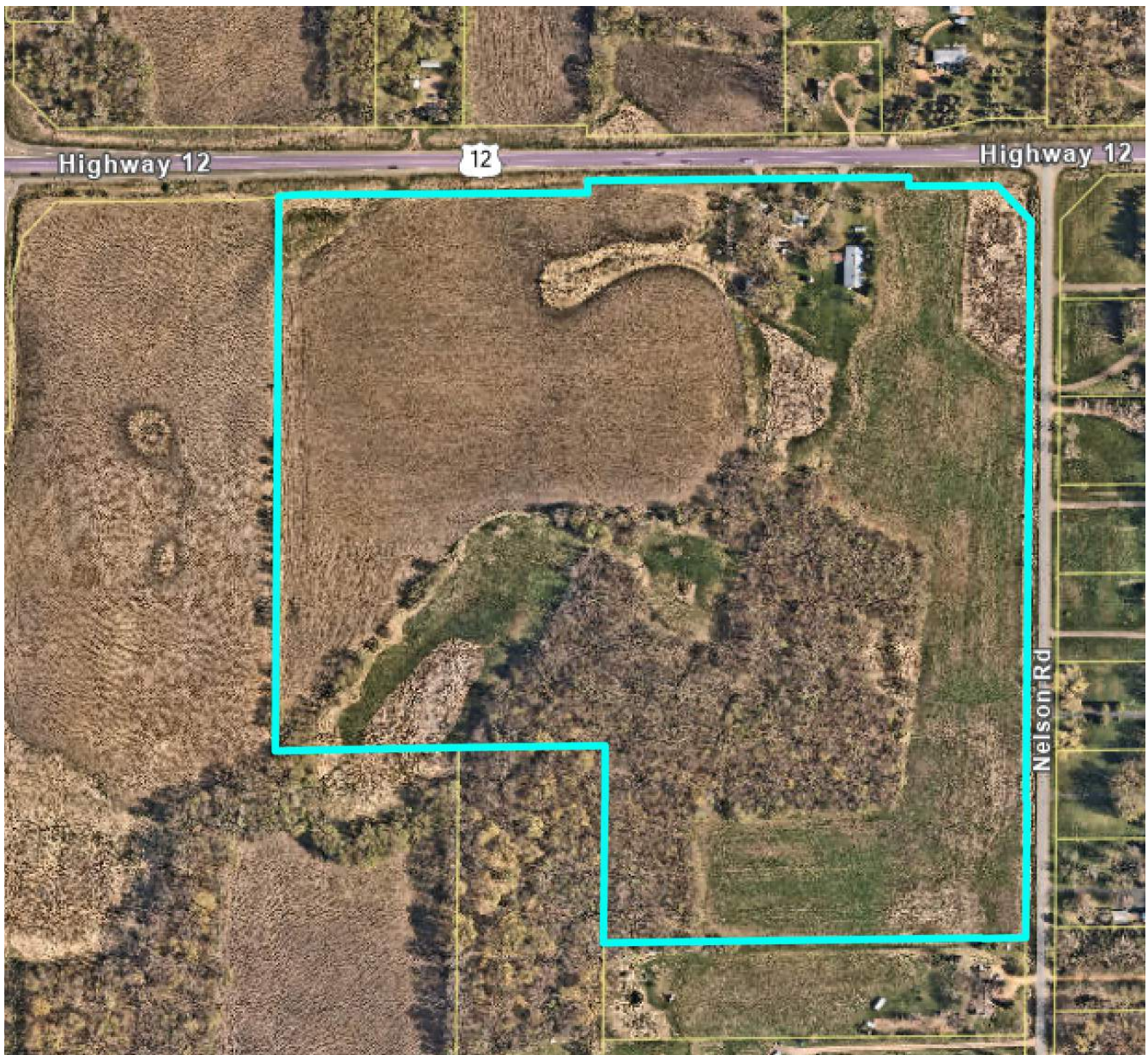
Property Information: 9285 Highway 12

Zoning: Agriculture

Comprehensive Plan: Agriculture/Urban Commercial

Acreage: ~58 acres

9285 Highway 12



UPDATE:

The City reviewed the proposed development in January 2023 and provided feedback and comments to the developer and staff. Following the meeting, the City has worked with the developer to make revisions to the plans in order to address comments provided by the City and its consultants (see attached review letters and applicant responses attached to this report). In addition, the City has been working with MNDOT to identify an acceptable access plan. The City has also prepared the parameters for the PUD that would allow for the subject development. The following summary points detail the changes, outstanding comments and general considerations that should be noted by the Planning Commission:

- One of the primary issues that was identified during the initial review of this projects relates to the proposed access to the site. The City noted that it strongly discourages any commercial connection to Nelson Rd. To support this standing, the City noted that the site development standards contain the following prohibition:

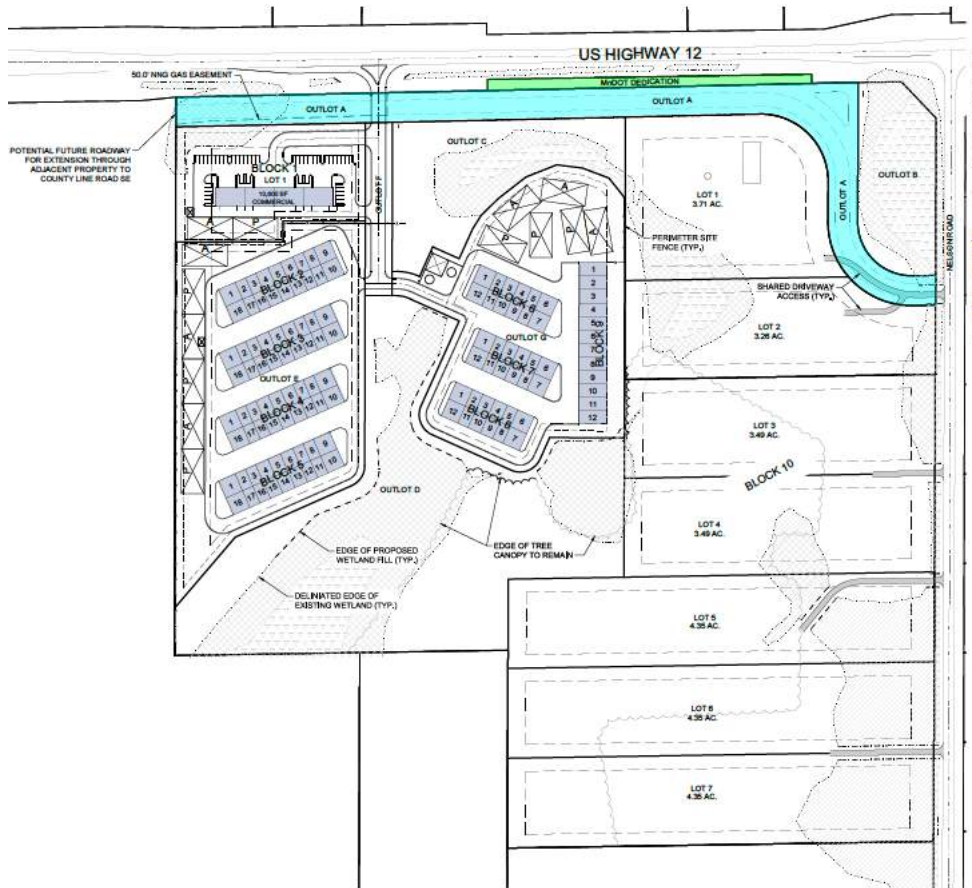
530.11. - Lot standards.

Subd. 10. *Access streets.* Streets servicing a commercial-light industrial area must have direct access to a collector street or higher-capacity street. **No street servicing commercial-light industrial establishments may have access to local residential streets nor may business-oriented traffic be routed or directed to local residential streets.**

The City discussed the possibility of providing for a future frontage road across the property to provide for a future east/west connection to County Line Road. The applicant is agreeable to providing this future ROW and has revised the drawings to provide for this future access. The City met with MNDOT and offered that this future ROW would be acquired by the City and allow for a future frontage road. This would likely allow Nelson Road residents to access the future roundabout at County Line Road and Highway 12 without having to go onto Highway 12. Following additional review by MNDOT, they have agreed to allow the proposed right in/right out as a temporary access that would need to be removed at the time a frontage road can be constructed to County Line Rd. However, MNDOT also wants a full frontage road connection made to Nelson Road within the within the right of way shown on the revised plans. This is not in keeping with the City's ordinances and increases the number of turning movements made from west bound Highway 12 onto Nelson Road (left turn off of Highway 12 onto Nelson Road from westbound Highway 12 lane). This turning movement is considered the most dangerous based on crash data.

Staff has asked to meet with MNDOT again relating to this issue noting the aforementioned points. Staff is seeking additional discussion from Planning Commission relating to the access issue. Staff has considered additional solutions should MNDOT not agree to allow the access as proposed. This would include looking at a one way street option (westbound traffic only) that would allow a right turn only off of Nelson Road and into the commercial development.

- Based on the proposed revisions, the applicant has reduced the number of residential properties to seven from eight. This reduction will allow for the future frontage road right of way and eliminate the lot directly adjacent to Highway 12.



- The applicant has revised the plans to add additional detail to the proposed 10,000 SF commercial building. The City has reviewed the updated site plan related information and generally finds that it meets applicable standards. The City reviewed parking relating to the proposed commercial and made the following findings:
 - 1 space per 250 SF (10,600 - 10.6 x 4 = 43 parking spaces)
43 parking spaces required
49 parking spaces provided
- The applicant has provided photometric and lighting plans for the proposed development. The lighting plans have been reviewed by the City and found to meet all applicable requirements. The lights proposed by the applicant are cut-off type fixtures that will have limited visibility outside of the proposed development. One exception is with the proposed commercial building. The City will want to see light poles within the proposed parking lot that will provide adequate light levels to the entire parking area.

- The applicant has provided architectural plans and building elevations for all buildings within the proposed development. The City notes that the architectural standards adopted for the Urban Commercial zoning district state the following:

Subd. 3. *Design standards.*

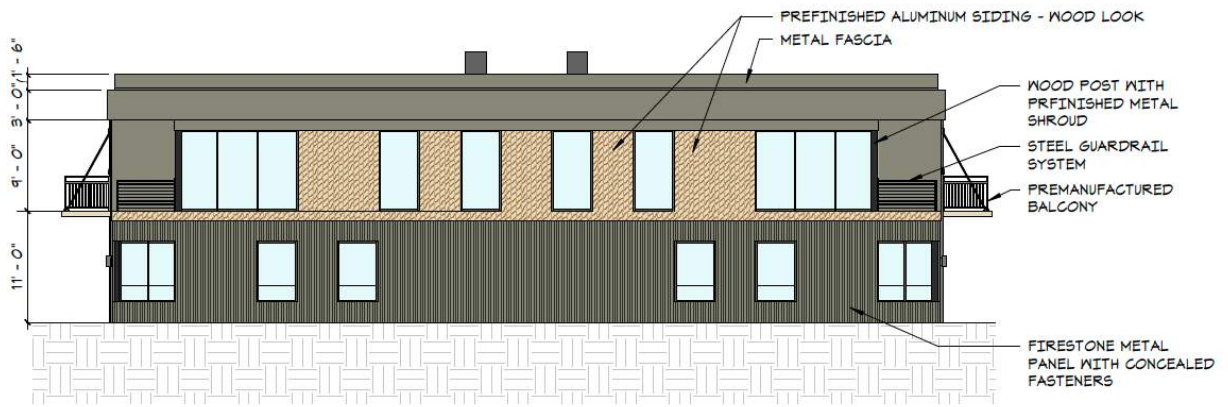
- (a) Height. The maximum height of all buildings must not exceed the lesser of 2½ stories or 35 feet. This height limitation does not apply to farm buildings, grain elevators, silos, windmills, elevator legs, cooling towers, water towers, chimneys and smokestacks, church spires, or electric transmission lines.
- (b) Allowed materials for principal buildings. Principal commercial or industrial buildings in the commercial/industrial zoning district shall use the following materials on their exterior facades:
 - (1) Brick;
 - (2) Natural stone or stone veneers;
 - (3) Decorative concrete block (color impregnated with a split faced, robbed, or textured surface;
 - (4) Glass curtain wall panels;
 - (5) Stucco or synthetic stucco;
 - (6) Exterior insulation and finish systems (EIFS).
- (c) All exterior vertical surfaces must be treated as a front and have an equally attractive or same fascia on all sides of the structure.
- (d) Mechanical protrusions. All necessary mechanical protrusions visible to the exterior must be screened or painted in a manner so they are not visually obvious and are compatible with the surrounding development. Satisfaction of this requirement must be demonstrated by the screening of the equipment in such a manner that it is not visible from a point six feet above any common property line or street right-of-way. Screening must consist of either a parapet wall along the roof's edge or by an opaque screen constructed of the same material as the building's primary vertical exposed exterior finish. The zoning administrator may determine that the equipment may be painted a neutral earth tone color, or color deemed similar by the zoning administrator or must be designed to be compatible with the architectural treatment of the principal building, which will satisfy the screening requirement. All mechanical protrusions must be highlighted on the site plan.

(Added, Ord. No. 2006-09)

Subd. 4. *New materials.* The city recognizes that technologies change and new products are continually available which may not be listed as allowed under these building design requirements. If an applicant wishes to utilize a non-listed material, an application to amend the zoning code text must be submitted per the requirements of city Code. In reviewing such an application to consider including a new material to the list of allowed materials in subdivision 3(b), the city will consider the following:

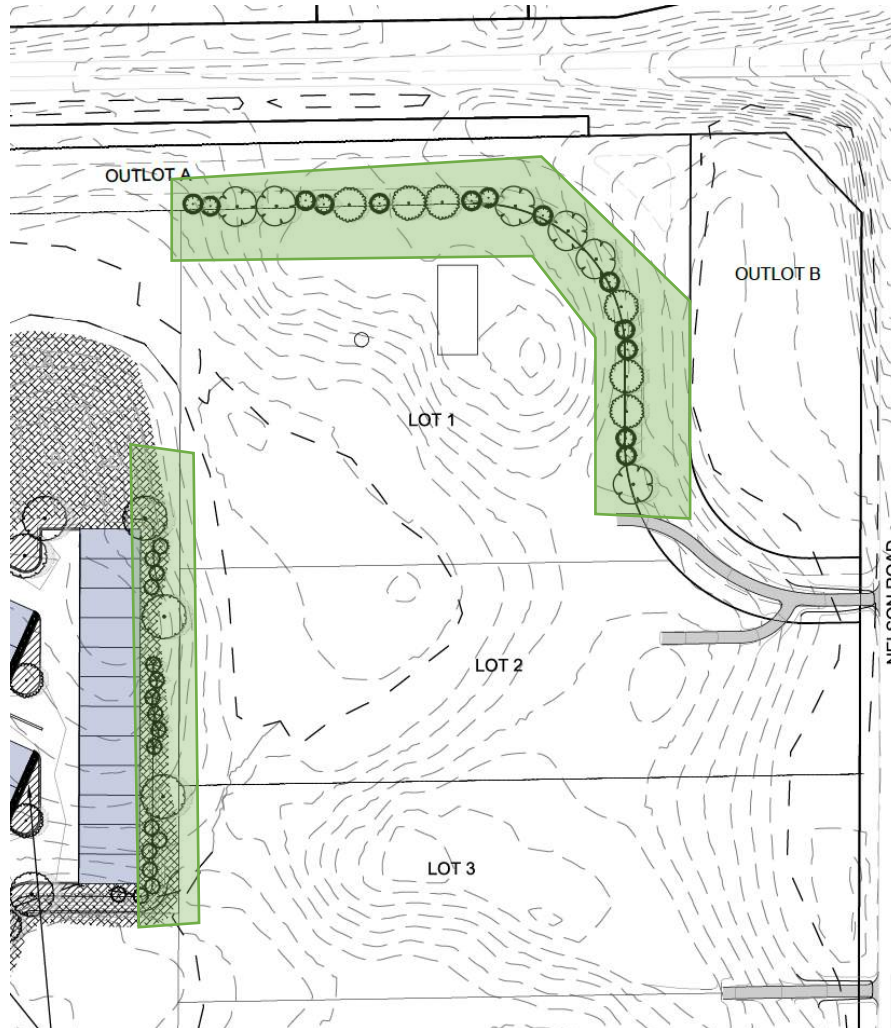
- (a) Is the proposed material of sufficient quality to ensure on-going maintenance will not be of concern (applicant should provide detailed information on the proposed product and its history of use);
- (b) Will the style, color, and appearance of the proposed product integrate with adjacent commercial/industrial properties and other materials currently allowed within the commercial/industrial zoning district;
- (c) Will the style, color, and appearance of the proposed product be acceptable in cases when visible from residential units on adjacent properties.

The applicant is proposing to construct the buildings using primarily metal paneling (see illustrative elevation below). The City will request examples of the proposed materials. The proposed architecture is consistent throughout the development, utilizes significant amounts of glass and glazed surfaces and appears to meet the design intent of the ordinance. Staff will be seeking additional discussion and direction from Planning Commission relating to the proposed building materials and architecture.



4 **MOTOR CONDOS - SIDE ELEVATION**
A3.1 3/32" = 1'-0"

- The applicant does not currently identify and mechanical equipment on the plans. All mechanical equipment will need to be shown and if proposed to be on the exterior of the building or roof, will need to be screened from view.
- A preliminary plat has now been submitted. The PP will need to be updated to reflect the City's standard 10' perimeter drainage and utility easements and provide all lot dimensions, easements and similar information.
- The City has reviewed the plans with the Delano Fire Chief. The applicant has provided an exhibit (attached to this report) which provides fire truck simulated turning movements which verify fire apparatus access. The City will want to further define no parking areas (using signage and striping) within the auto condo and commercial development areas. This will be further detailed as the project progresses.
- The applicant has provided an updated landscape plan for the proposed development. The plan provides a high level of landscaping around the commercial building, entrance drives and within the proposed lifestyle garage condos. The City does believe that additional landscaping screening may be warranted in several locations shown below.



- The City has prepared a draft of the Planned Development standards that would apply to the commercial development. It should be noted that the residential lots will be subject to the requirements of the AG-Agriculture physical and lot standards. The City will need to waive the minimum lot width for requirement for Lots 5, 6 and 7. The following Planned Development Standards would be incorporated in the Planned Development Agreement:

PLANNED DEVELOPMENT STANDARDS

(A) *Permitted uses.* Permitted uses of land or buildings, as hereinafter listed, shall be permitted in the Planned Development under the conditions specified. No building or lot shall be devoted to any use other than a use permitted hereunder:

- (1) Any use not expressly permitted but consistent with the intent of the district which, by approval of the city council, shall be permitted except for eating and

drinking establishments, coffee shops and similar food service businesses which shall not be permitted.

(2) Office and retail sales.

(3) Contractor and construction supply shops conducted wholly within an enclosed building, including but not limited to plumbing, HVAC, roofing and electrical with a principle building or office.

(4) Light manufacturing, production, processing, assembly, fabricating, servicing and repairing of materials, goods or products with no outdoor storage.

(5) Auto Condos in accordance with the following provisions:

- i. Auto Condos shall be for the private storage and private leisure by the owner(s) of such space. No commercial activities shall be permitted in the auto condos, nor shall any auto condo be ever occupied for residential purposes and the following additional conditions shall apply:
 - a) Individual signs shall not be permitted for any units within the development.
 - b) No outdoor storage will be permitted.
 - c) Parking shall be permitted only in the designated parking spaces.

(B) *Accessory uses.* None.

(C) *Conditional uses.* The following conditional uses may be permitted by action of the city council pursuant to subsections [520.09](#), [520.11](#) and [520.13](#):

- (1) Telecommunications towers approved pursuant to [section 540](#) of this Code;
- (2) Any other use determined by the city council to be similar in character to a permitted use, but potentially requiring special conditions to ensure compatibility.

(D) *Physical standards.*

- (1) Minimum Lot Size: per approved general plan
- (2) Minimum Lot Width: per approved general plan
- (3) Front Yard: 50 feet

- (4) Side Yard: 20 feet
- (5) Corner Side Yard: 20 feet
- (6) Rear Yard: 30 feet
- (7) Residential Properties: 40 feet
- (8) Agriculture Properties: 40 feet
- (9) Parking and Drive Aisle:
 - a. Front Yard: 15 feet
 - b. Side Yard: 5 feet
 - c. Rear Yard: 5 feet

Discussion:

The City reviewed a concept site plan in 2022 for this property and provided feedback to the applicant and staff. The applicant considered the feedback and direction and has now made a formal application to develop the subject property. The applicant has revised the proposed development to include eight (8) single-family lots along Nelson Road, 120-130 lifestyle auto condominiums and 10,600 SF of commercial/retail space. The proposed lifestyle auto condominiums would be developed to allow for vehicle storage and would include amenities associated with this type of product (i.e., loft, bathroom, etc.). The applicant and City would work to establish regulations to prohibit overnight stays and limit and define any larger events (i.e., auto show) that could occur on the property. It is acknowledged that the proposed lifestyle auto condominiums would produce less traffic, require fewer public resources, and provide more market valuation than the initial commercial/warehouse development. The facility would be secured, professionally managed, and generally concealed from Highway 12 and Nelson Road.

Approximately 10,600 SF of commercial retail is proposed by the applicant. This area would have Highway 12 frontage and visibility consistent with the City's Comprehensive Plan. The City has reviewed the initial site plan layout and is generally in agreement with the proposed layout. The City would require a more detailed site plan specifically for the proposed Lot 9 development. The plan will need to provide more detail relating to allowed and proposed uses, parking and drive aisle dimensions, landscaping and lighting. In addition, the City will want to consider how/if the site could or would accommodate retail sales such as a drive thru and restaurant. A more detailed parking analysis would need to be completed

In order for the City to consider approval of the proposed plan, the following approvals and steps would be required:

1. Amend the Comprehensive Plan.

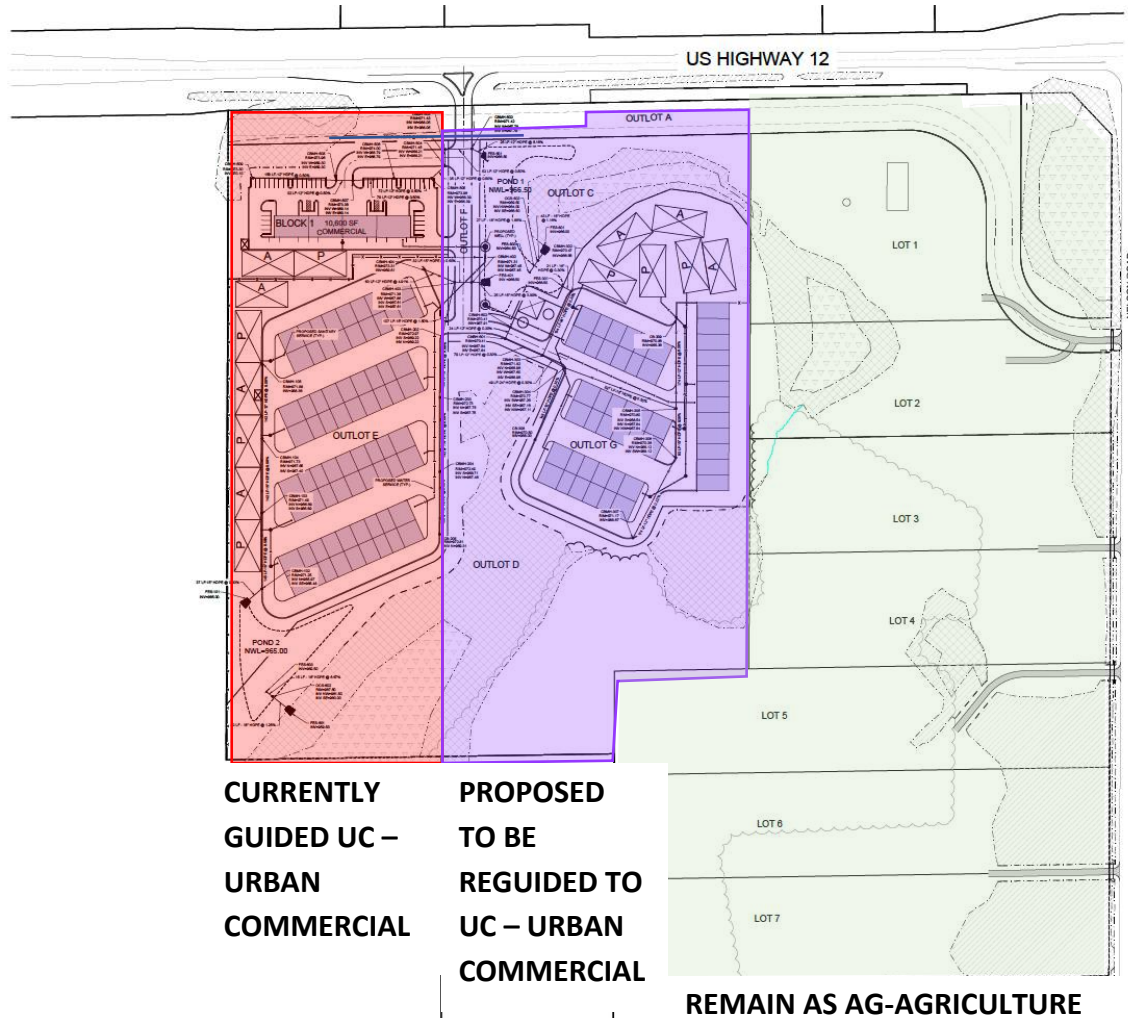
- a. This would re-guide a portion of the property (~16 acres) from AG-Agriculture to Urban Commercial.
2. Rezone that portion of the property indicated from AG-Agriculture to Urban Commercial.
3. Consider Site Plan Review.
4. Consider a Conditional Use Permit to allow a planned unit commercial development on the subject property.
5. Consider Preliminary Plat approval.
6. Consider Final Plat approval.

The following land uses are proposed by the applicant (plans attached):

- Commercial office/retail (10,600 SF on ~ 2.33 acres)
- Lifestyle auto condominiums (102 units on ~ 11.98 acres)
- Seven (7) Residential Lots (approximately 3.26 – 4.35 acres each)
- Frontage Road ROW (3.22 acres)

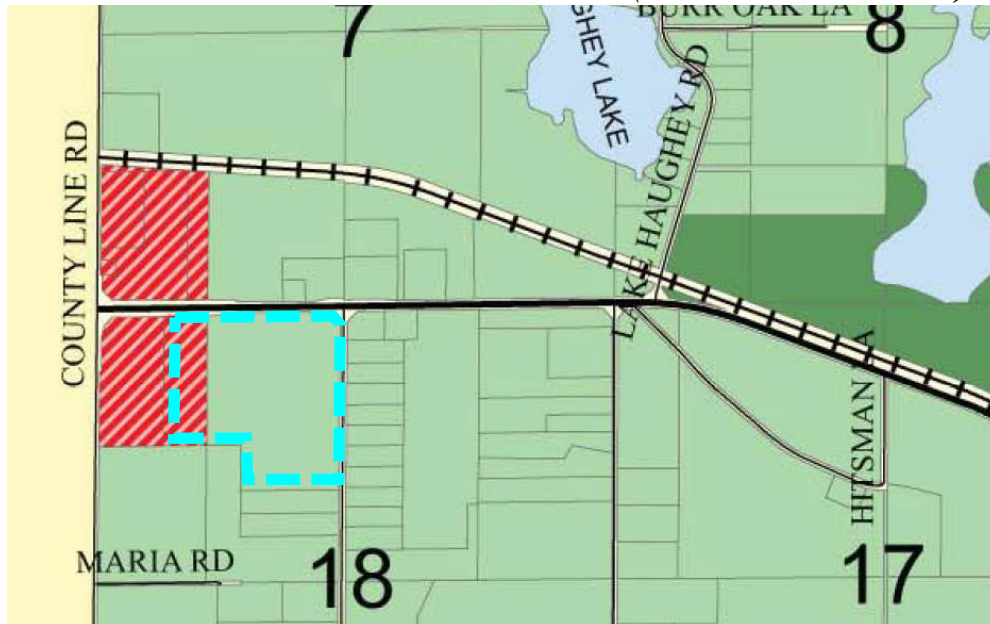
There are several key points of information that should be noted relating to the proposed development:

- The entire property is currently zoned AG-Agriculture.
- A portion of the property, approximately 12 acres on the west side is guided by the City's Comprehensive Land Use Plan for Urban Commercial.
- The applicant is asking the City to consider rezoning a larger portion of the property from AG-Agriculture to Urban Commercial (~16 acres).



- The remainder of the property (~28 acres) would remain as AG-Agriculture.
- In order for the property to be rezoned, the City would first need to approve a Comprehensive Plan Amendment. A comprehensive plan amendment process would be subject to approval by the Metropolitan Council.

• **COMPREHENSIVE PLAN MAP (CLIP OF TOTAL MAP)**



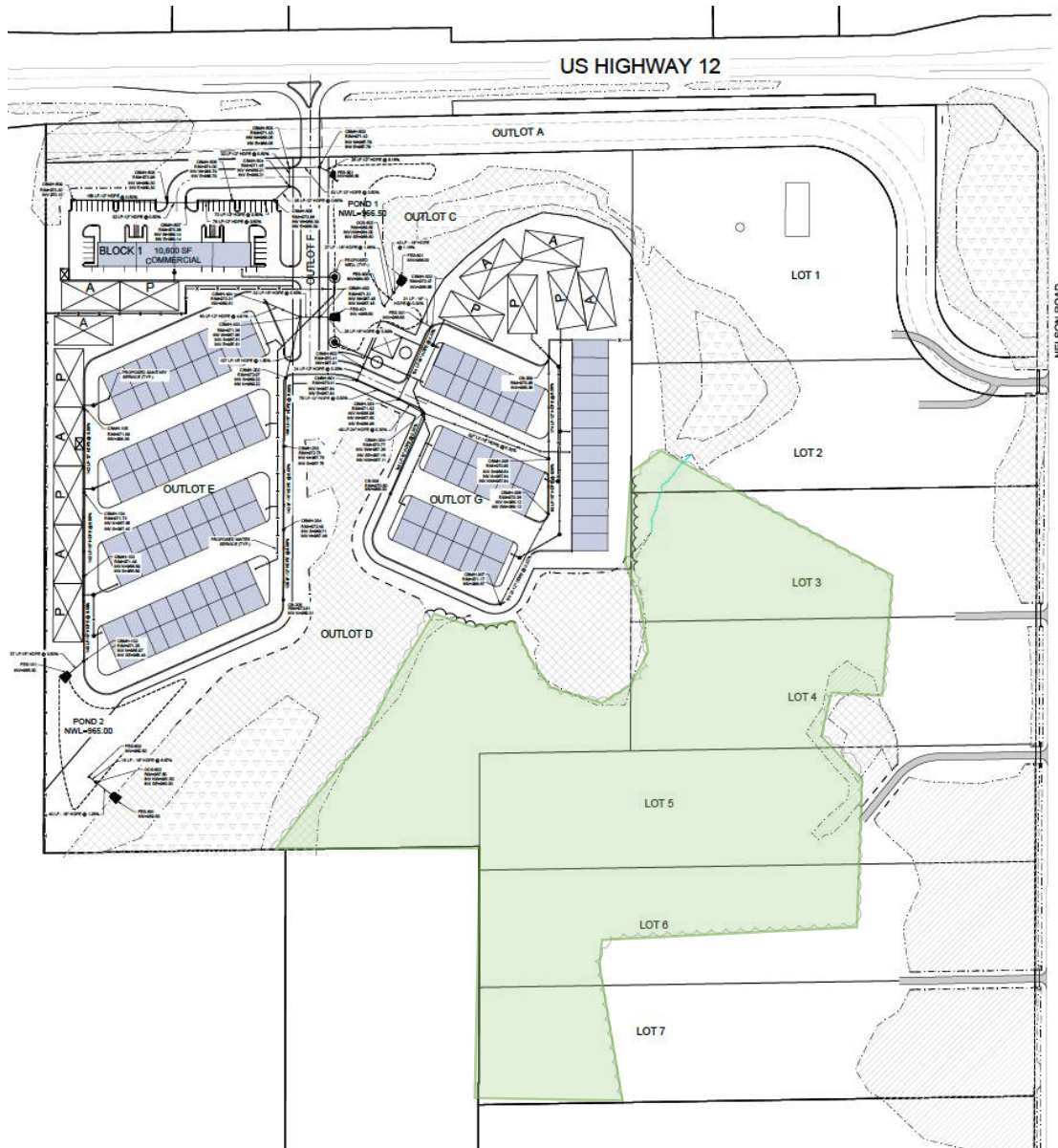
- The initial submittal considered access to Nelson Road and the applicant was notified that the City would not support any commercial access to Nelson Road. The plans submitted include a right in/right out only onto Highway 12 with a future frontage road connection to the west that could connect to County Line Road (shown on site plan). MNDOT has reviewed the plan and provided formal comments to the City and applicant. MNDOT has commented that they would recommend access to this site be provided via a connection to Nelson Road or County Line Road rather than a right in/right out as proposed. As a result of the MNDOT review, access has not been finalized. The issue of access will need to be resolved prior to the City finalizing a decision relating to this application.
- The applicant has provided the City with a traffic study as requested. The study indicates that the proposed development would generate a total of 7 entering and 1 exiting trips during the morning traffic peak and 2 entering and 7 exiting during the afternoon traffic peak hour. A total of 68 trips per day would be generated by the proposed auto condos and commercial retail uses. This would equate to approximately 1 trip every 7 minutes during peak times and approximately 2 trips per hour throughout the remainder of the day (see table below). It is noted that the proposed right in/right out would allow access to the proposed development until such time as a frontage road could be constructed to connect to County Line Road. Staff is seeking direction relating to access and the overall proposal from the Planning Commission and City Council.

Table 1
Trip Generation Zeglin Property

Land Use	ITE Code	AM Peak Hour		PM Peak Hour		Daily Trips
		Enter	Exit	Enter	Exit	
Mini Warehouse (120 Units)	151	1 Trip	0 Trips	1 Trip	1 Trip	19 Trips
Commercial/LI (10.6 k-sf)	110	6 Trips	1 Trips	1 Trip	6 Trips	49 Trips
TOTAL		8 Trips		9 Trips		68 Trips

- Following discussion with Planning and Council, the applicant has revised the plans to include eight (8) residential lots along Nelson Rd. The proposed residential lots mirror the lots across Nelson Rd. to the east. Development of the west side of Nelson into residential lots that are similar to those in existence would provide additional buffering of the proposed commercial development. It is noted that the existing residential lots directly across and on the east side of Nelson Road from the subject property have approximately 200 LF of frontage. The City's current ordinance would require 250 LF of frontage for all lots greater than 3.49 acres. This would include Lots 1, 2 & 6-8. Lots 3-5 comply with the frontage requirements. The applicant could revise the plan to reduce the acreage of the proposed lots or increase the frontages to meet the specified requirements. The applicant is attempting to mirror the lots across the street; however, those lots are all less than 3.49 acres in size.
- The City noted in its initial review that the potential wetland impact associated with the development of driveways to serve the lots was a concern. The applicant has revised the plans to show the extent of the wetland impacts and is now proposing to utilize shared driveways for Lots 1&2, 3&4 and 6&7. Utilization of shared driveways would reduce the wetland impacts and the number of driveways located along Nelson Road.
- The applicant is proposing to provide on-site sewer (septic) and on-site water to serve the proposed development. The applicant has provided a report indicating how the development of on-site water would serve the fire suppression requirements for the proposed development. The City is continuing to evaluate and study the possibility of establishing a municipal well/water service in the location of the Urban Commercial to serve commercial development on the north and south sides of Highway 12. Should the proposed development move forward, the City will provide additional information relating to a municipal well to Council for consideration.
- The applicant has completed a wetland delineation for the property. The City initially provided feedback to the applicant relating to the potential wetland impacts. The applicant is proposing to mitigate the impacted wetlands utilizing the purchase of wetland credits. Approximately 31.36 acres of the subject property is wetlands. The applicant is proposing to impact 2.48 acres of the total wetland area (> 8%). The potential wetland impacts would need to be reviewed and approved by the City. Further review of the wetland mitigation plan would occur following an initial City approval.

- Stormwater management has been proposed for the development. The City has completed an initial review of the stormwater management plan and provided high level comments to the applicant. The proposal includes 3 stormwater ponds that would accommodate stormwater generated from the commercial portion of the proposed development. The City will continue to work with the applicant and would anticipate a more detailed submittal and calculations relating to the stormwater plan should the project proceed to final plat.
- The applicant is proposing to preserve a large portion of the mature trees on the property (see gray area below).



The applicant has provided the City with a robust package of details and information relating to the proposed development. The proposed lifestyle auto condominium illustrations appear to utilize high quality building materials and a sophisticated design that is unique for this product in the metro area. There are many details that will need to be finalized prior to the City being able to formally act on the proposed site development and preliminary plat. Due to the significant number of pieces included in the proposed site development, staff would like additional direction and feedback relating to the overall proposal so that additional review and direction can be provided to the applicant. In particular, staff is seeking additional review and direction relating to the proposed land use, expansion of the UC – Urban Commercial zoning district, access to the property and the development of the residential properties on Nelson Road. It is anticipated that following discussion and additional direction, the City would be able to work with the applicant to finalize the preliminary plans and bring back additional information and any revisions recommended for further consideration. It is also anticipated that the City and applicant will have further discussions with MNDOT relating to their recommended access for this property.

Recommendation:

The applicant is seeking feedback from the Planning Commission pertaining to the proposed development.

Attachments:

1. Application
2. Applicants Narrative
3. Site Plan Package
4. Building Illustrations
5. Wetland Mitigation Plans
6. Access Assessment
7. Traffic Report

***Mixed Use Development Application for a 58 Acre Parcel
Located on State Hwy 12 & Nelson Road in Independence for a
Business Park, Upscale Life-Style Auto Condo & Residential
Home Development***

Proposed Application Summary:

Our proposed development by SH Ventures, Inc. (Stoddard Companies) is for a Mixed Use Development to include commercial space, upscale Life-Style Auto Condos and eight single family home lots abutting Nelson Road. We are proposing to create one commercial building along the south border of Hwy 12, approximately eight buildings containing approximately 120-130 auto condos and eight residential lots along Nelson Road of approximately 3.21-4.35 acres each and 200+ feet of Nelson Road frontage. The property is Zoned Agricultural on this Future Comprehensive Planned property of Urban Commercial and Agricultural.

We are applying for:

- Comp Plan Amendment to add a portion of the property from Ag to Urban Commercial;
- Rezoning to allow the property to be rezoned;
- Conditional Use Permit to consider a PUD;
- Site Plan Review; and
- Preliminary Plat.

Since Our Concept Stage Application we have reduced the density, scale and mass of our proposed development significantly by omitting the two larger structures and increased our proposed residential lots from three lots to eight. We are proposing one commercial building over 10,000 sf on the property fronting Hwy 12, with our private, gated auto condo phase south of the commercial. We are proposing access to this phase of the development from Hwy 12 in our Legal 60 ft. wide MnDot open access area-refer to Exhibit #12. We have also added an area to permit commercial/auto condo access directly to County Line Road if we are able to work our something with our Western neighbor.

We additionally have enhanced the rural feel of our project by developing around the existing wetlands therefore allowing more space between buildings and have also increased our residential area to eight lots on 30.5 acres which is also 52.6% of the

58 acre site. Only residential access from the eight residential Lots is proposed on Nelson Road. We are proposing to keep the existing metal outbuilding and small concrete silo on Lot 1 while demolishing the existing house and other outbuildings in that area for our redevelopment.

Mixed Use Development Features:

Commercial: We are proposing one building of 10,600 sf to eventually be home to likely 1-3 businesses to serve the community.

Life-Style Auto Condos: We are proposing approximately 151,900 sf of 120-130 upscale Life-Style Auto Condos. Additional on-site parking will accommodate potential events about 6-8 times/year to be competitive in the marketplace. We will have supervised event parking and private security for events and a Condo Homeowners Association will be required of all members to control storage, prohibit overnight stays and maintain grounds maintenance.

Residential Lots: We have increased the amount of proposed residential Lots to eight total to enhance the rural nature of the surrounding area to be compatible with the residential Lots on the east side of Nelson Road. Lots will have a minimum of 200 ft. Nelson Road frontage and be similar or larger than the residential Lots on the east side of Nelson Road.

City of Independence Comprehensive Plan:

The City of Independence Comprehensive Plan 2040 states various Visions, Goals and Policies it desires with its land planning including:

- Promoting & attracting quality industrial and commercial business to the City of Independence;
- Working to update official controls and ordinances to accommodate the projected comprehensive plan; and
- Maintaining an economic balance and enhance the geographic advantage of the City.

Our proposed development helps realize Independence's Vision by:

- Entitling our proposed mixed use development paving the way for constructing buildings featuring commercial with employment opportunities, auto condo and residential home options on underutilized land on this transportation corridor;
- Taking geographic advantage of the abutting City of Delano's commercial offerings to our western Independence location; and
- Increase the Tax Base by a conservative number of \$47,000,000.00 for the community which potentially helps offset other property taxes citywide.

There has been a strong demand for similar projects and our proposed development aims to raise the bar and deliver beautifully designed commercial and auto condo space designed by award winning ESG Architects with ample open outdoor space. This market is burgeoning, and our proposed development may add north of \$47,000,000.00 to the local tax base, while not requiring municipal sewer service. Municipal water service would be a strong plus however project is possible with water well service.

We have designed ample open space with setbacks from abutting properties to the east, west and south. We have attached architectural renderings and civil engineering plans in attached Exhibits. This Narrative and accompanying Plans & Exhibits will further detail items as requested by City requirements with additional information on Legal Site access, market research, septic system design, life safety and traffic analysis 7 trip generation also in attached Exhibits 1 thru 12.

Application Information:

- (a) **General Information:** Names and addresses of existing landowner and applicant found on attached Exhibit #1- PID's & Legal Description of Property and on City Application Form. Professional Consultants for our proposed development include:

Architect:	ESG Architecture & Design 500 S. Washington Ave. #1080 Minneapolis, MN 55415
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Civil Engineer: Anderson Engineering of MN, LLC
13605 1st Ave. N #100
Plymouth, MN 55441

Traffic Engineer: Swing Traffic Solutions
4290 Norwood Land North
Plymouth, MN 55442

Attorney/Legal: Messerli & Kramer
1400 Fifth Street Towers
100 South Fifth St.
Minneapolis, MN 55402

Title Company: Custom Homes Builders Title LLC
10850 Old County Rd 15 #100
Plymouth, MN 55441

The place-setter name of “Independence Mixed Use Development” will be used until next Application stage. Applicant and landowner confirm that a valid purchase agreement exists, and a title commitment is provided from Custom Homes Builders Title LLC of Plymouth, MN.

- (b) **Present Status:** Address of vacant land is 9285 US Highway No. 12 with property Identification Information and legal addresses attached. Existing Zoning is Agricultural and is guided for Urban Commercial and Ag in the Comprehensive Land Use Plan. The Comprehensive Land Use Plan is attached as Exhibit #2 and the Wetland Investigation Report and Notice of Decision is attached as Exhibit #3. An updated site survey is being currently obtained.
- (c) **Site Conditions:** Site conditions depict a relatively flat site currently being farmed with a farmhouse, outbuildings and large wooded area. Topography, drainage patterns and any other significant features per Survey.
- (d) **Architectural Renderings of Proposed Development:** Refer to attached Exhibit #4 -Architectural Renderings by ESG Architects depicting Upscale Auto Condos and Commercial featuring external façade showcasing glass panel, metal architectural ornamentation and more for low maintenance requirements.

- (e) **Civil Engineering & Landscape Plan by Anderson Engineering:** Please refer to attached Exhibit #5 for plans featuring very large areas of open space being left untouched in our development per our site plan.
- (f) **Utilities Plan:** A Utilities plan will is attached with the sanitary septic designed by a registered engineer & designer. Ample space exists for this low usage need and while City water would be a huge advantage to this development, water can be provided for its water needs including fire safety refer to Exhibit #9- Water Well Capacity Memorandum by Anderson Engineering.
- (g) **Legal Opinion of Hwy 12 MnDot Access:** A Legal Opinion Letter regarding our Hwy 12 MnDot Access by Messerli & Kramer is provided in Exhibit #6.
- (h) **Trip Generation Analysis:** A Trip Generation Analysis concluding the limited amount of traffic generated by our proposed development along with a right-in/right/out safety access lane per STS Traffic Solutions, Vern Swing, PE is attached in Exhibit #12.
- (i) **Additional Exhibits:** Additional Exhibits are attached for information including Market Research, Septic System, Concept Life Safety Fire Suppression Systems and more in attached Exhibits.
- (j) **Additional Information:** Applicant will forward any additional information as requested by City in prompt fashion.

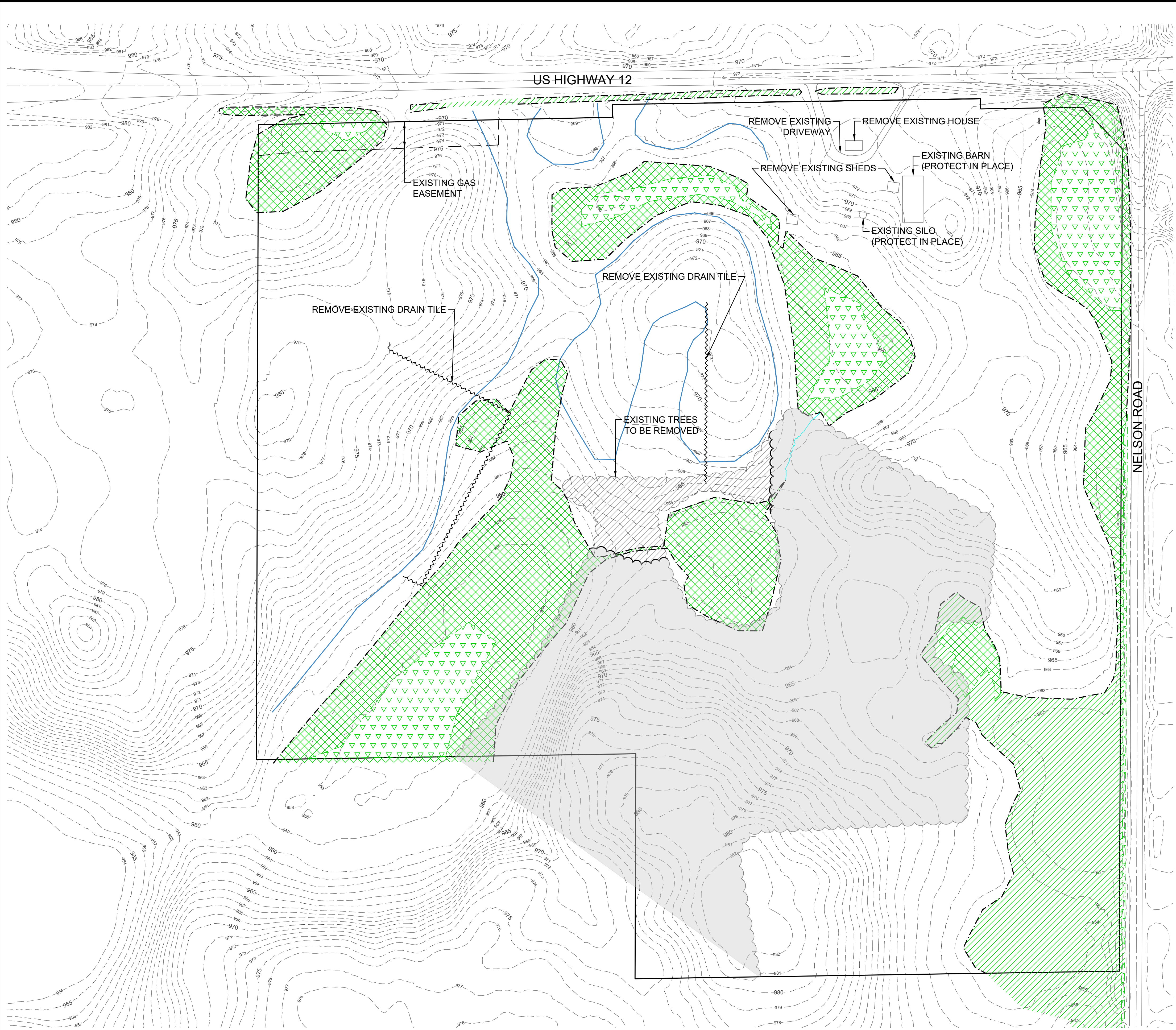
Refer to next page for list of Exhibits

ATTACHED EXHIBITS:

- 1. Property Identification (PID) & Legal Description of Property;**
- 2. Comprehensive Land Use Plan Map;**
- 3. Wetland Investigation Report & Notice of Decision (NOD);**
- 4. Architectural Renderings by ESG Architects;**
- 5. Civil Engineering & Landscape Plan Package by Anderson Engineering;**
- 6. Legal Opinion Letter Regarding Hwy 12 MnDot Access by Messerli & Kramer Law Firm;**
- 7. Market Research/Opinion Letter by Maxfield Research & Consulting;**
- 8. Neighboring Nelson Road Map of SFH Lot Sizes Depicting Proposed New Lots Compatibility with Surrounding Neighborhood;**
- 9. Water Well Capacity Memorandum by Anderson Engineering;**
- 10. Septic System Primary & Alternate Size Design by Homestead Septic;**
- 11. Concept Life Safety Fire Suppression System Options from Summit Fire Consulting; and**
- 12. Trip Generation Analysis by STS Traffic Solutions, Vern Swing, PE.**

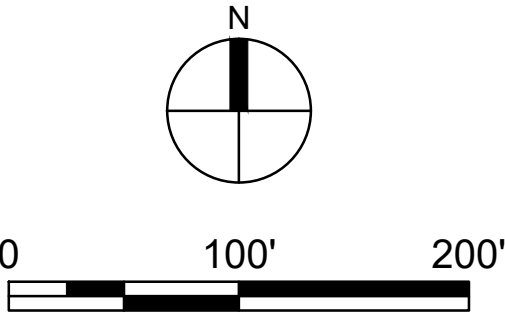
Cc: Mark Kaltas
Amber Simon

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LEGEND

- PROPERTY LIMITS
- EXISTING WETLAND
- EXISTING DRAIN TILE
- EXISTING STORM SEWER
- EXISTING GAS MAIN
- EXISTING OVERHEAD WIRES
- EXISTING TELEPHONE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING STORM SEWER INLET
- REMOVE EXISTING UTILITY LINE
- REMOVE EXISTING TREES
- TYPE 1 WETLAND
- TYPE 2 WETLAND
- TYPE 3 WETLAND
- UPLAND (AS FIELD DESIGNATED BY CERTIFIED SEPTIC DESIGNER DALE DENN - SEE REPORT)



ANDERSON

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Anderson Engineering of Minnesota, LLC

MIXED USE
DEVELOPMENT

INDEPENDENCE,
MINNESOTA

STODDARD COMPANIES

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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EXISTING
CONDITIONS &
DEMOLITION PLAN

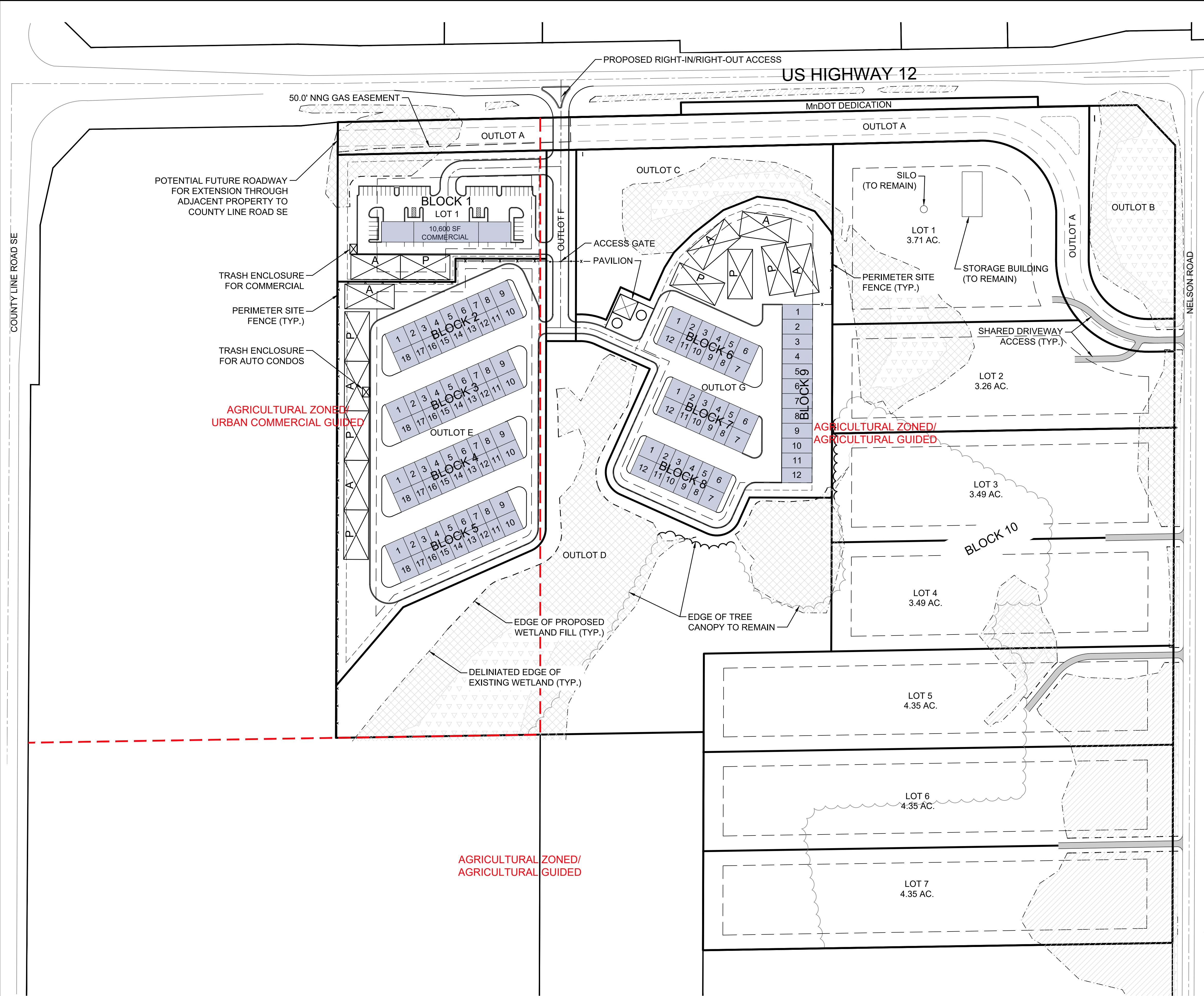
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LEGEND

	DRAIN FIELD (PRIMARY)
	DRAIN FIELD (ALTERNATE)
	BUILDING SETBACK
	PARKING SETBACK
	EX. WETLAND BOUNDARY
	WETLAND FILL BOUNDARY
	PARCEL/LOT BOUNDARY
	ZONING BOUNDARY
	WETLAND (TYPE 1)
	WETLAND (TYPE 2)
	WETLAND (TYPE 3)

PARCEL AREA	
BLOCK 1, LOT 1	2.33 ACRES
BLOCK 2, 3, 4, 5	0.51 ACRES EACH
LOT 1, 9, 10, 18	1,639 SF
LOT 2, 3, 7, 8, 11, 12, 16, 17	1,024 SF
LOT 4, 5, 6, 13, 14, 15	1,229 SF
BLOCK 6, 7, 8	
LOT 1, 6, 7, 12	1,639 SF
LOT 2, 6, 8, 11	1,024 SF
LOT 3, 4, 9, 10	1,229 SF
BLOCK 9	
LOT 1 TO 12	1,844 SF
BLOCK 10	
27.00 ACRES	
OUTLOT A	3.23 ACRES
OUTLOT B	1.58 ACRES
OUTLOT C	2.37 ACRES
OUTLOT D	8.92 ACRES
OUTLOT E	6.38 ACRES
OUTLOT F	0.53 ACRES
OUTLOT G	5.61 ACRES
MnDOT DEDICATION	0.34 ACRES
TOTAL	58.28 ACRES

PARKING
BLOCK 10
N/A - RESIDENTIAL LOTS

BLOCK 1
AUTO STALLS PROVIDED = 49
10,600 SF COMMERCIAL @ 4.0 PER 1,000 SF = 42

BLOCKS 2 THROUGH 5 - 72 AUTOCONDO UNITS

PARKING STALLS PROVIDED = 151
1,700 LF PERIMETER PARKING @ 1 STALL/25 LF = 68
2,088 LF BUILDING FACE @ 1 STALL/25 LF = 83

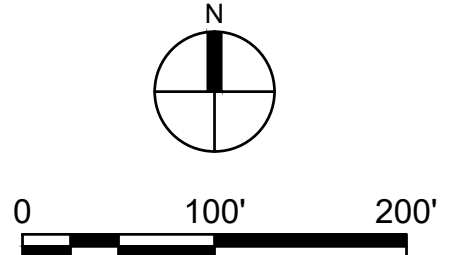
BLOCKS 6 THROUGH 9 - 48 AUTOCONDO UNITS
AUTO STALLS PROVIDED = 97
1,000 LF PERIMETER PARKING @ 1 STALL/25 LF = 40
1,443 LF BUILDING FACE @ 1 STALL/25 LF = 57

SETBACKS

PARKING/DRIVEWAY
COMMERCIAL/AUTOCONDO
FRONT 15' (FROM FRONTAGE ROAD)
SIDE 20' (FROM AGRICULTURE)
REAR 5' (ADJACENT COMMERCIAL)
REAR 40' (FROM RESIDENTIAL)

BUILDING
COMMERCIAL/AUTOCONDO
FRONT 50' (FROM FRONTAGE ROAD)
SIDE 30' (FROM RESIDENTIAL)
REAR 40'
SIDE 80' (FROM AGRICULTURE)

RESIDENTIAL
FRONT 85' (FROM NELSON ROAD CENTERLINE)
SIDE 30'
REAR 40'



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REVISION LOG		
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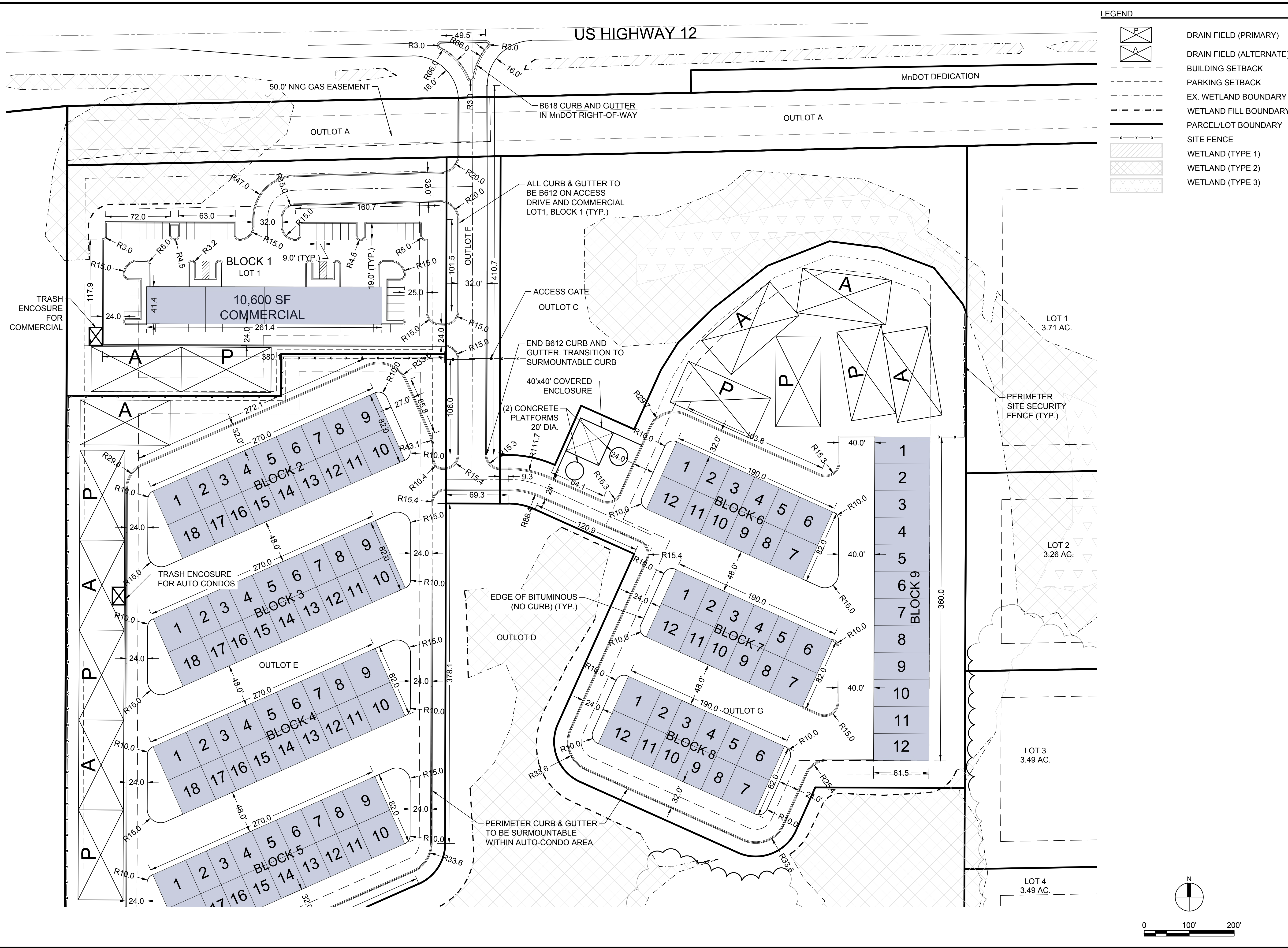
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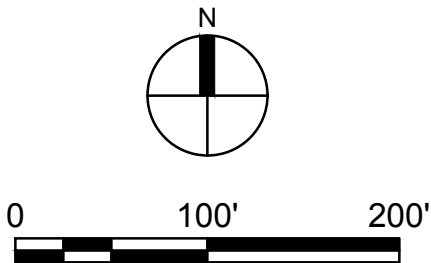
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LEGEND	
	DRAIN FIELD (PRIMARY)
	DRAIN FIELD (ALTERNATE)
	BUILDING SETBACK
	PARKING SETBACK
	EX. WETLAND BOUNDARY
	WETLAND FILL BOUNDARY
	PARCEL/LOT BOUNDARY
	SITE FENCE
	WETLAND (TYPE 1)
	WETLAND (TYPE 2)
	WETLAND (TYPE 3)





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REVISION LOG

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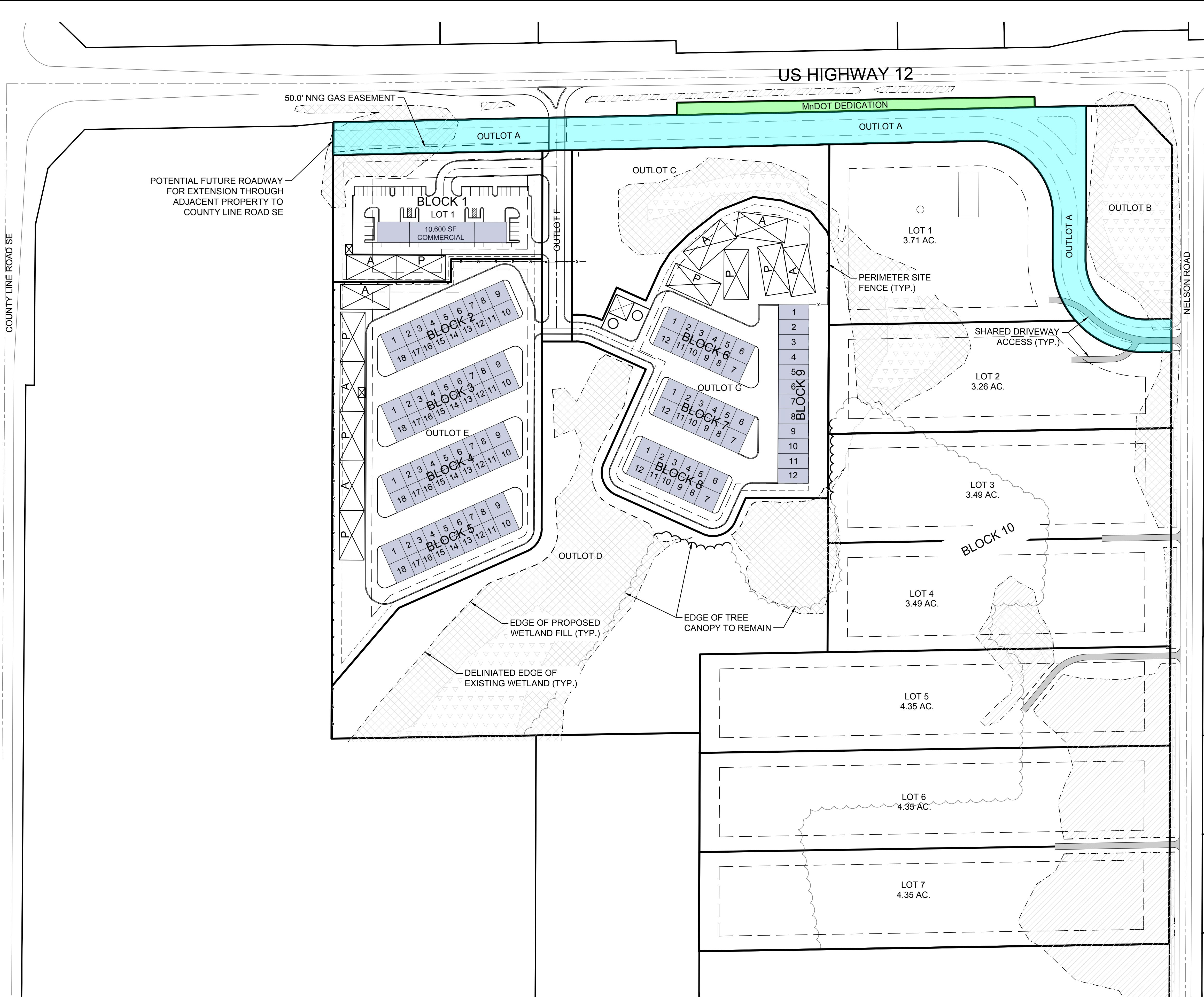
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LEGEND

	DRAIN FIELD (PRIMARY)
	DRAIN FIELD (ALTERNATE)
	BUILDING SETBACK
	PARKING SETBACK
	EX. WETLAND BOUNDARY
	WETLAND FILL BOUNDARY
	PARCEL/LOT BOUNDARY
	DEDICATED TO MNDOT (0.34 ACRES)
	FRONTAGE ROAD (3.22 ACRES)
	WETLAND (TYPE 1)
	WETLAND (TYPE 2)
	WETLAND (TYPE 3)

PARCEL AREAS		
RESIDENTIAL	27.00 ACRES	46.3%
AUTOCONDOS	11.98 ACRES	20.6%
COMMON SPACE/		
OUTLOTS	13.40 ACRES	23.0%
FRONTAGE RD	3.23 ACRES	5.5%
COMMERCIAL	2.33 ACRES	4.0%
Mndot DEDICATION	0.34 ACRES	0.6%
TOTAL	58.28 ACRES	

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REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

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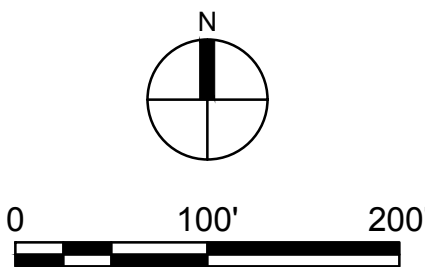
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DEDICATION

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COUNTY LINE ROAD SE

US HIGHWAY 12

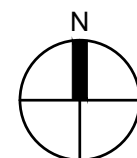
NELSON ROAD

LEGEND

- PROPERTY LIMITS
PROPOSED WATERMAIN
FUTURE SANITARY SEWER
PROPOSED STORM SEWER
PROPOSED FENCE
PRIMARY SEPTIC DRAINFIELD
ALTERNATE SEPTIC DRAINFIELD
PROPOSED WATER WELL
PROPOSED SANITARY MANHOLE
PROPOSED STORM INLETS
FES

GENERAL NOTES:

- ALL WATERMAIN ARE REQUIRED TO BE INSTALLED AT 7.5 FEET MINIMUM DEPTH FROM FINISHED GRADE AND PROVIDE MINIMUM 10' HORIZONTAL AND 18" VERTICAL SEPARATION OF ALL WATERMAIN CROSSINGS FROM STORM OR SANITARY SEWER. WATERMAIN TO BE INSULATED PER C.E.A.M. SPECIFICATIONS, WHERE COVER DEPTHS CANNOT BE ACHIEVED. CONTRACTOR SHALL CONTACT THE ENGINEER IF THERE ARE AREAS WHERE MINIMUM COVER DEPTH CANNOT BE MET.
- ALL NEW NON METALLIC SANITARY AND WATER SERVICE PIPE SHALL HAVE TRACER WIRE, PER SPECIFICATIONS.
- SUBSTITUTIONS FROM INFO. SHOWN HERON SHALL BE REVIEWED AND APPROVED BY THE ENGINEER OF RECORD.
- ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED IN ACCORDANCE WITH MINNESOTA RULES, CHAPTER 4714, SECTION 1109.0.
- PVC SANITARY SEWER MUST MEET ASTM STANDARDS LISTED IN MINNESOTA RULES, CHAPTER 4714, TABLE 701.1 AND INSTALLATION STANDARD 1. FITTINGS MUST COMPLY WITH ASTM D1866, D2665, OR F794 RESPECTIVELY. JOINTS MUST BE APPROVED MECHANICAL OR PUSH-ON UTILIZING ELASTOMERIC SEAL, OR SOLVENT WELDING USING ASTM F656 AND ASTM D2564. THE INSTALLATION MUST COMPLY WITH ASTM D232.
- HDPE STORM SEWERS MUST MEET ASTM F714 (SEE MINNESOTA RULES, CHAPTER 4714, SECTION 1102.4 AND INSTALLATION STANDARD 1), WATER TIGHT JOINTS MUST BE USED AT ALL CONNECTIONS, INCLUDING STRUCTURES. THE INSTALLATION MUST COMPLY WITH ASTM D2321.
- WATER SERVICE MATERIALS SHALL BE PVC C900. CONTRACTOR SHALL VERIFY EXISTING SERVICE SIZE AND MATERIALS PRIOR TO CONSTRUCTION. ALL INSTALLED PIPE WILL REQUIRE TESTING PER CITY AND STATE REQUIREMENTS. THIS WILL INCLUDE, AT A MINIMUM, CONDUCTIVITY AND PRESSURE TESTING, AS WELL AS TESTING FOR BACTERIA AND PROPER DISINFECTION. FOLLOW C.E.A.M. STANDARD SPECIFICATIONS UNLESS NOTED OTHERWISE. INSTALL TRACER WIRE, JOINT RESTRAINTS AND THRUST BLOCKING ON ALL WATERMAIN PIPE AND FITTING, PER CITY OF MEDINA STANDARDS.
- SANITARY SEWER PIPE MATERIALS SHALL BE PVC SDR 26. PIPE SHALL BE INSULATED PER CITY AND C.E.A.M. STANDARDS WHERE 7.5 FOOT COVER DEPTHS ARE NOT ACHIEVED. THIS APPLIES TO ALL PROPOSED NEW PIPE FOR THIS PROJECT. ALL SANITARY SEWER PIPE WILL REQUIRE TESTING PER CITY AND C.E.A.M. STANDARDS. THIS WILL INCLUDE, AT A MINIMUM, LEAKAGE AND ALIGNMENT TESTING.
- ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE WATER TIGHT. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATER TIGHT CONNECTIONS TO MANHOLES AND CATCH BASINS.



0 100' 200'



ANDERSON

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Anderson Engineering of Minnesota, LLC

MIXED USE DEVELOPMENT

INDEPENDENCE,
MINNESOTA

STODDARD COMPANIES

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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UTILITY PLAN

DRAWING NO.

C200

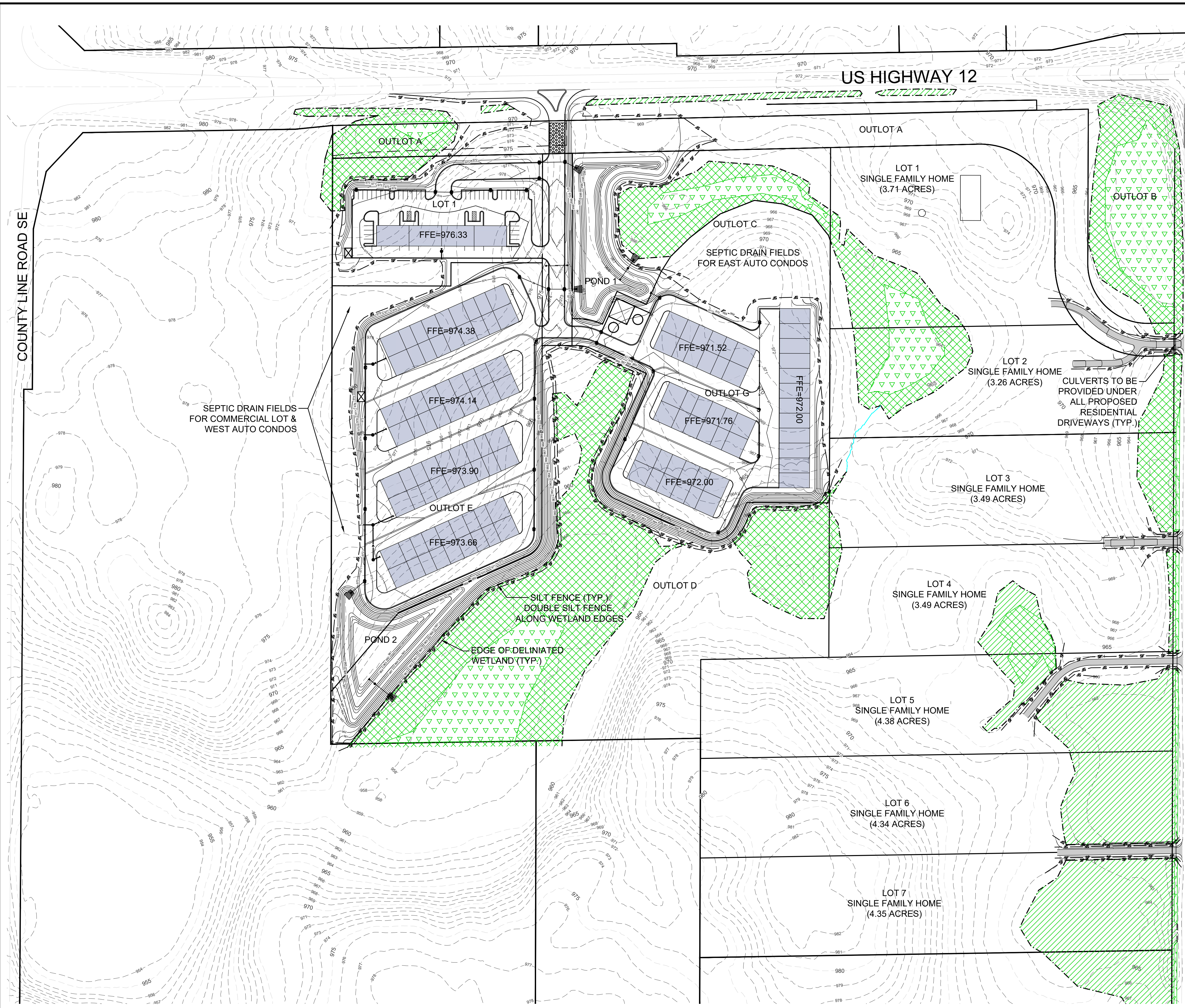
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LEGEND

—	PROPERTY LIMITS
- - - 966 - - -	EXISTING MINOR CONTOUR
- - - 965 - - -	EXISTING MAJOR CONTOUR
— 966 —	PROPOSED MINOR CONTOUR
— 965 —	PROPOSED MAJOR CONTOUR
x 959.4	EXISTING SPOT ELEVATION
⇒	DRAINAGE ARROW
—●—●—●—	BIOROLL EROSION CONTROL
—s—s—s—	SILT FENCE
—●—●—●—	PROPOSED STORM SEWER
⊙	INLET PROTECTION
■	RIP RAP FLARED END
■	ROCK CONSTRUCTION ENTRANCE

NOTES

1. ALL EROSION CONTROL MEASURES CALLED FOR ON THESE PLANS, WHICH MAY INCLUDE SILT FENCE, SEDIMENTATION BASINS OR TEMPORARY SEDIMENT TRAPS, SHALL BE CONSTRUCTED AND SERVICEABLE IN THE FOLLOWING ORDER:

- A. ROCK CONSTRUCTION ENTRANCES.
- B. SILT FENCE.
- C. COMMON EXCAVATION AND EMBANKMENT (GRADING)
- D. SEED AND MULCH OR SOD.
- E. BIOROLL BARRIERS IN FINISHED GRADED AREAS.
- F. INLET AND OUTLET FACILITIES SUBSEQUENT TO STORM SEWER WORK.

GRADING CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL EROSION CONTROL MEASURES AS MAY BE SHOWN ON THESE PLANS OR SPECIFICATIONS. GRADING CONTRACTOR SHALL IMPLEMENT ANY ADDITIONAL EROSION CONTROL MEASURES AS MAY BE REQUIRED TO PROTECT ADJACENT PROPERTY.

2. ALL EROSION CONTROL FACILITIES SHALL BE MAINTAINED BY THE CONTRACTOR DURING GRADING OPERATIONS. ANY TEMPORARY FACILITIES WHICH ARE TO BE REMOVED AS CALLED FOR ON THESE PLANS AND SPECIFICATIONS SHALL BE REMOVED BY THE GRADING CONTRACTOR WHEN DIRECTED BY THE ENGINEER. THE GRADING CONTRACTOR SHALL THEN RESTORE THE SUBSEQUENTLY DISTURBED AREA IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS.

3. ALL DISTURBED AREAS, EXCEPT AREAS TO BE PAVED AND/OR SPECIFICALLY DESIGNED BY A LANDSCAPE PLAN, SHALL BE COVERED WITH A MINIMUM 6" OF TOP SOIL. ALL DISTURBED AREAS SHALL BE SEEDED & MULCHED AT THE PRESCRIBED RATES WITHIN 72 HOURS OF FINAL GRADING UNLESS OTHERWISE NOTED.

4. ALL EXPOSED SOIL AREAS WITH A CONTINUOUS POSITIVE SLOPE WITHIN 200 LINEAL FEET OF ANY SURFACE WATER, MUST HAVE TEMPORARY EROSION PROTECTION OR PERMANENT COVER FOR THE EXPOSED SOIL AREAS YEAR ROUND, ACCORDING TO THE FOLLOWING TABLE OF SLOPES AND TIME FRAMES.

5. IT IS REQUIRED THAT SOILS TRACKED FROM THE SITE BY MOTOR VEHICLES BE CLEANED DAILY FROM PAVED ROADWAY SURFACES THROUGHOUT THE DURATION OF CONSTRUCTION.

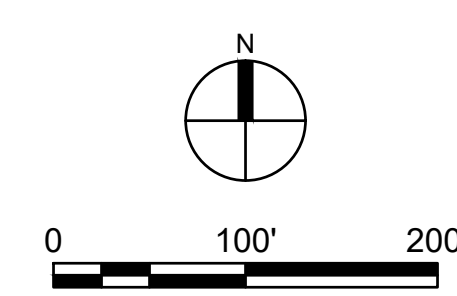
6. ALL REQUIREMENTS OF THE LOCAL WATERSHED DISTRICT SHALL BE SATISFIED PER THE APPROVED PERMIT.

7. ALL EROSION & SEDIMENT CONTROL MEASURES SHOWN ON THIS PLAN AND IMPLEMENTED IN THE FIELD AS DIRECTED BY THE ENGINEER SHALL CONFORM TO THE MPCA'S "PROTECTING WATER QUALITY IN URBAN AREAS: BEST MANAGEMENT PRACTICES FOR MINNESOTA".

8. ALL SOLID WASTE/ CONSTRUCTION DEBRIS SHALL BE DISPOSED OF IN ACCORDANCE WITH MPCA REQUIREMENTS. HAZARDOUS MATERIALS SHALL BE STORED/ DISPOSED OF IN COMPLIANCE WITH MPCA REGULATIONS

9. INLET SEDIMENTATION CONTROL IS TO BE PROVIDED TO ALL STORM SEWER CATCH BASINS THROUGHOUT CONSTRUCTION. MEASURES APPLIED SHALL COMPLY WITH BEST MANAGEMENT PRACTICES FOR MINNESOTA AND APPLICATIONS OF NPDES PHASE II AS APPROPRIATE FOR PHASE OF CONSTRUCTION.

10. CONTRACTOR SHALL PREVENT SOIL LOSS DURING CONSTRUCTION DUE TO WIND EROSION. DUST SHALL BE SUPPRESSED THROUGH THE APPLICATION OF WATER AS DEEMED NECESSARY BY THE CONTRACTOR, OR THROUGH EQUIVALENT BMP'S AS APPROVED BY THE ENGINEER.



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INDEPENDENCE, MINNESOTA

STODDARD COMPANIES

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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DRAWING TITLE

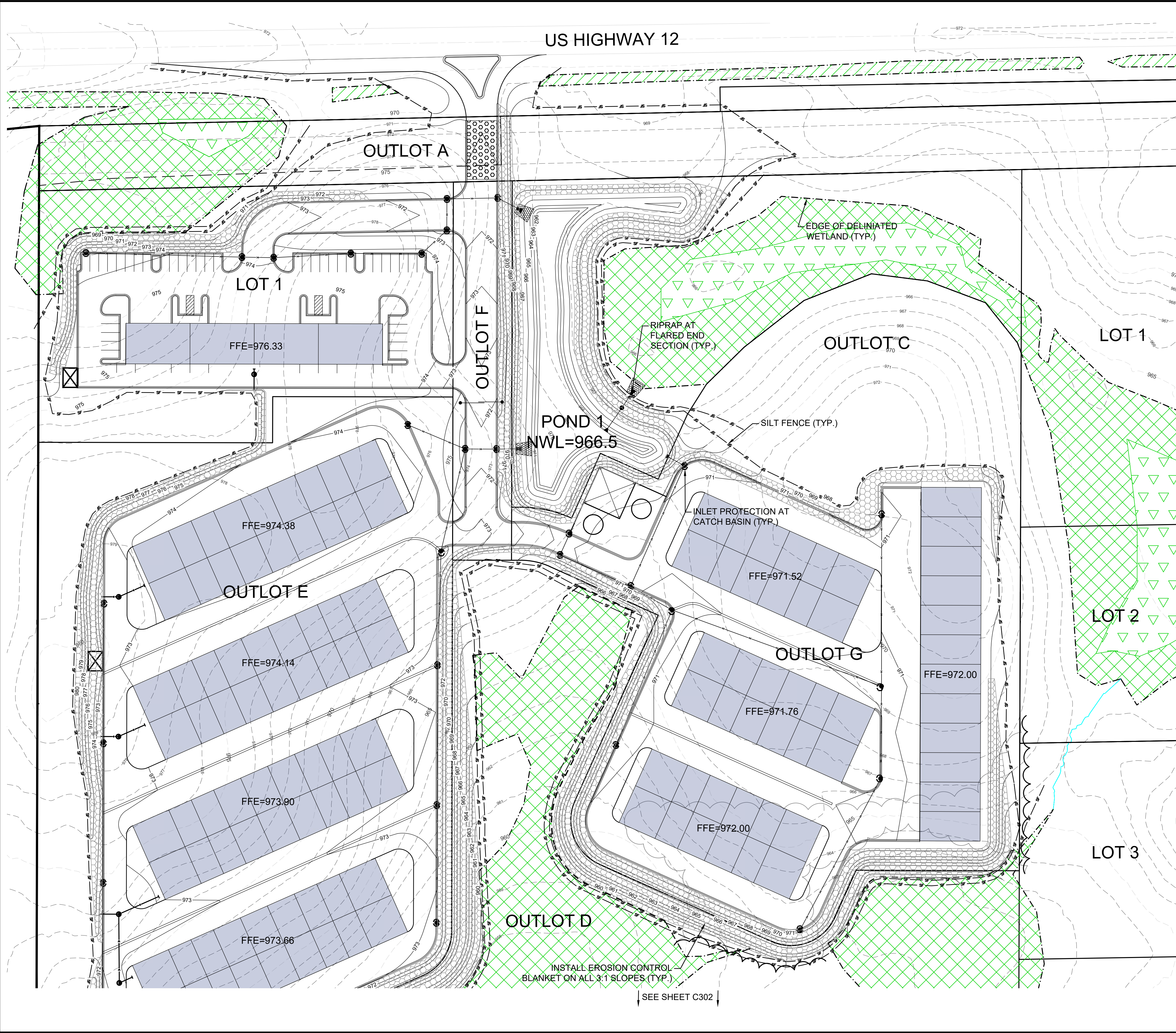
OVERALL EROSION CONTROL

DRAWING NO.

C300

PLOTTED: 02/10/2023	COMM. NO. 16915
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M:\\Miller



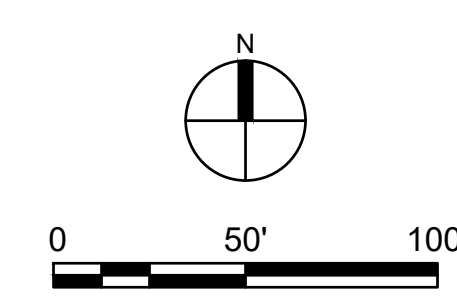
LEGEND


—	PROPERTY LIMITS
- - - 966 - - -	EXISTING MINOR CONTOUR
- - - 965 - - -	EXISTING MAJOR CONTOUR
— 966 —	PROPOSED MINOR CONTOUR
— 965 —	PROPOSED MAJOR CONTOUR
x 959.4	EXISTING SPOT ELEVATION
➔	DRAINAGE ARROW
=====	BIOROLL EROSION CONTROL
- - - - -	SILT FENCE
- - - - -	WETLAND DELINIATION PROPOSED
—	STORM SEWER
⊗	INLET PROTECTION
⊗	RIP RAP FLARED END
⊗	ROCK CONSTRUCTION ENTRANCE

NOTES

- ALL EROSION CONTROL MEASURES CALLED FOR ON THESE PLANS AND SPECIFICATIONS, WHICH MAY INCLUDE SILT FENCE, SEDIMENTATION BASINS OR TEMPORARY SEDIMENT TRAPS, SHALL BE CONSTRUCTED AND SERVICEABLE IN THE FOLLOWING ORDER:
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B. SILT FENCE.
C. COMMON EXCAVATION AND EMBANKMENT (GRADING)
D. SEED AND MULCH OR SOD.
E. BIO-ROLL BARRIERS IN FINISHED GRADED AREAS.
F. INLET AND OUTLET FACILITIES SUBSEQUENT TO STORM SEWER WORK.

GRADING CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL EROSION CONTROL MEASURES AS MAY BE SHOWN ON THESE PLANS OR SPECIFICATIONS. GRADING CONTRACTOR SHALL IMPLEMENT ANY ADDITIONAL EROSION CONTROL MEASURES AS MAY BE REQUIRED TO PROTECT ADJACENT PROPERTY.
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STODDARD COMPANIES

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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DRAWING TITLE

EROSION CONTROL PLAN (NORTH)

DRAWING NO.

C301

PLOTTED: 02/10/2023	COMM. NO. 16915
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Feb 10, 2023 - 12:39pm
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MMiller

COUNTY LINE ROAD SE

US HIGHWAY 12

OUTLOT A

OUTLOT B

OUTLOT C

OUTLOT D

EDGE OF DELINIATED
WETLAND (TYP.)

POND 1
NWL= 966.5

POND 2
NWL= 965.0

LOT 8

LOT 9

LOT 10

LOT 1

LOT 2

LOT 3

LOT 4

LOT 5

LOT 6

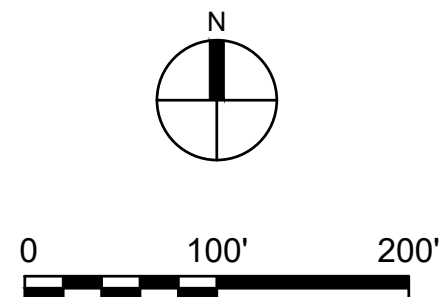
LOT 7

LEGEND

- PROPERTY LIMITS
- WETLAND DELINEATION LINE
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- DRAINAGE ARROW
- PROPOSED CONCRETE C&G
- SPOT ELEVATION
- FINISHED FLOOR ELEVATION

NOTE

- ALL DISTURBED AREAS TO BE SEEDED OR SODDED PER LANDSCAPE PLAN.
- PROPOSED CONTOURS AND SPOT ELEVATIONS ARE TO FINISHED GRADE.
- NO GRADED SLOPES SHALL EXCEED 3:1 (HOR:VER)
- FINISHED PAVEMENT SHALL BE FLUSH WITH EXISTING ADJACENT PAVEMENT.
- SEE WETLAND MITIGATION PLAN C500 FOR WETLAND IMPACTS.



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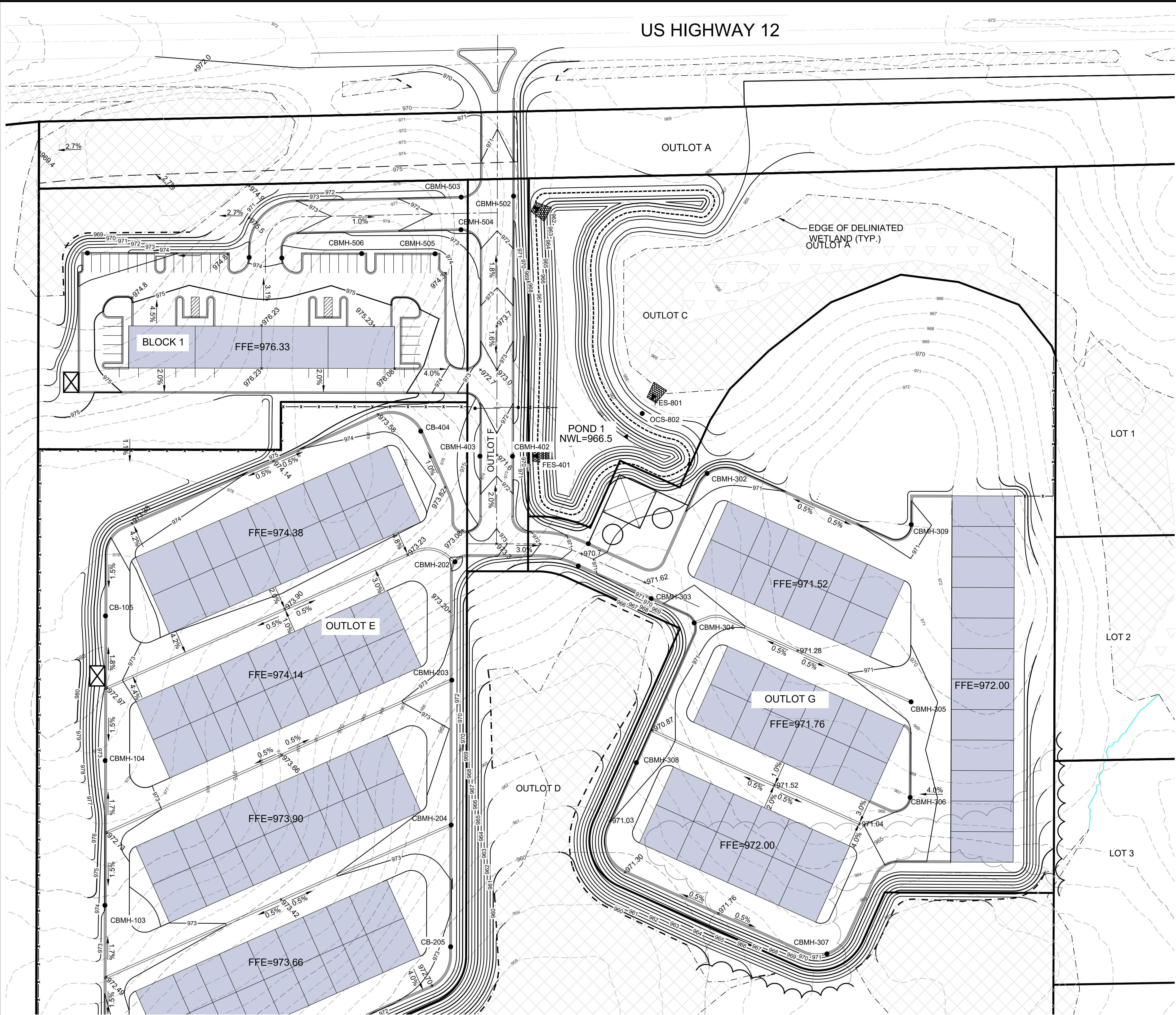
OVERALL GRADING
PLAN

DRAWING NO.

C400

PLOTTED: 02/10/2023	COMM. NO. 16915
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MMiller



LEGEND

- PROPERTY LIMITS
- WETLAND DELINEATION LINE
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- DRAINAGE ARROW
- PROPOSED CONCRETE C&G
- SPOT ELEVATION

NOTE

- ALL DISTURBED AREAS TO BE SEED OR SODDED PER LANDSCAPE PLAN.
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GRADING PLAN
NORTH

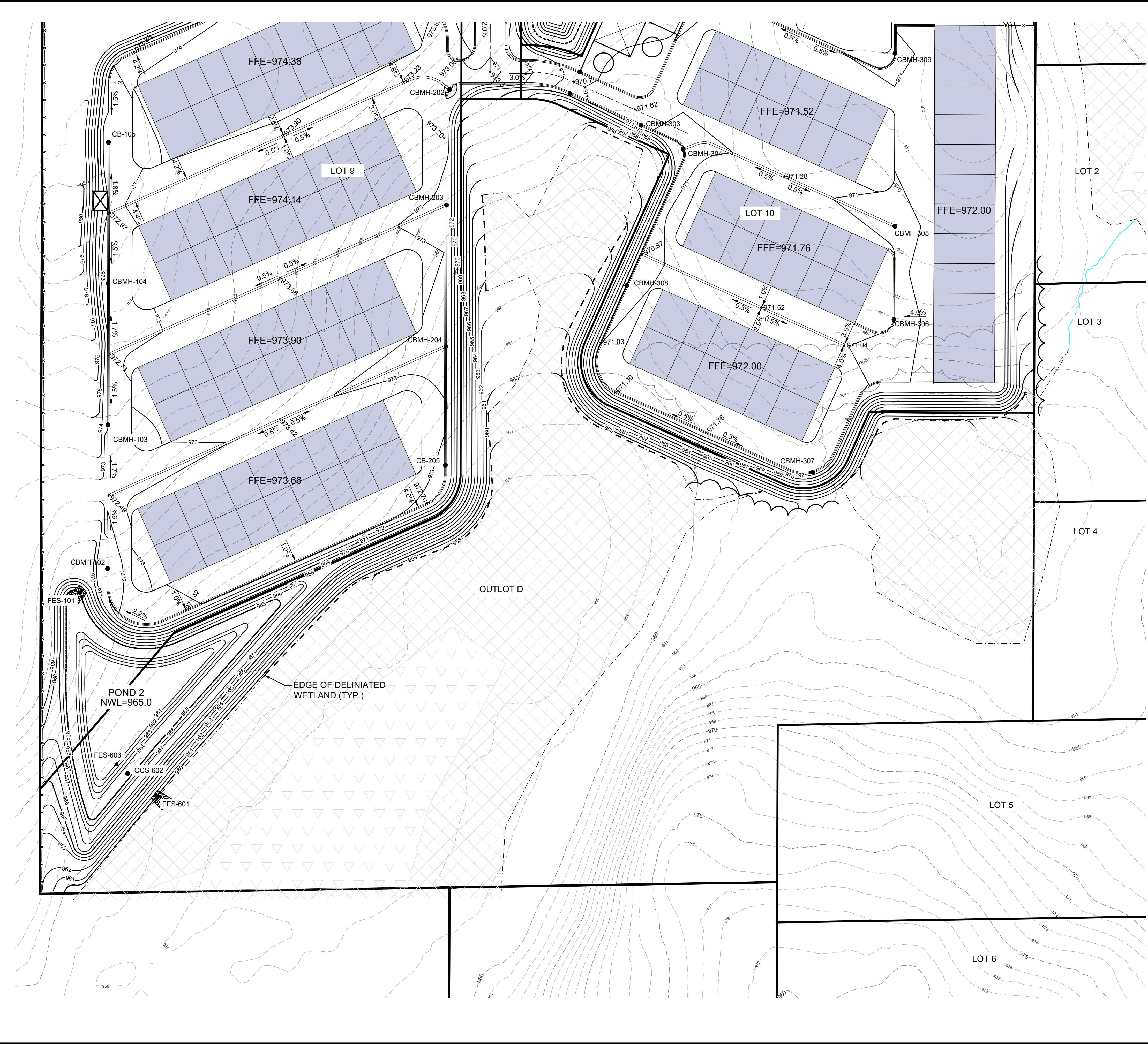
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02/10/2023

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LEGEND

PROPERTY LIMITS

WETLAND DELINEATION LINE

EXISTING MINOR CONTOUR

EXISTING MAJOR CONTOUR

PROPOSED MINOR CONTOUR

PROPOSED MAJOR CONTOUR

DRAINAGE ARROW

PROPOSED CONCRETE C&G

SPOT ELEVATION

NOTE

1.

ALL DISTURBED AREAS TO BE SEED OR SODDED PER LANDSCAPE PLAN.

2.

PROPOSED CONTOURS AND SPOT ELEVATIONS ARE TO FINISHED GRADE.

3.

NO GRADED SLOPES SHALL EXCEED 3:1 (HOR:VER)

4.

FINISHED PAVEMENT SHALL BE FLUSH WITH EXISTING ADJACENT PAVEMENT.

5.

SEE WETLAND MITIGATION PLAN C500 FOR WETLAND IMPACTS.

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED:
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DRAWN:
BN/MM

CHECKED BY:
LRK

DRAWING TITLE

GRADING PLAN
SOUTH

DRAWING NO.

C402

PLOTTED:
02/10/2023

COMM. NO.
16915

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DRAWN:
BN/MM

CHECKED BY:
LRK

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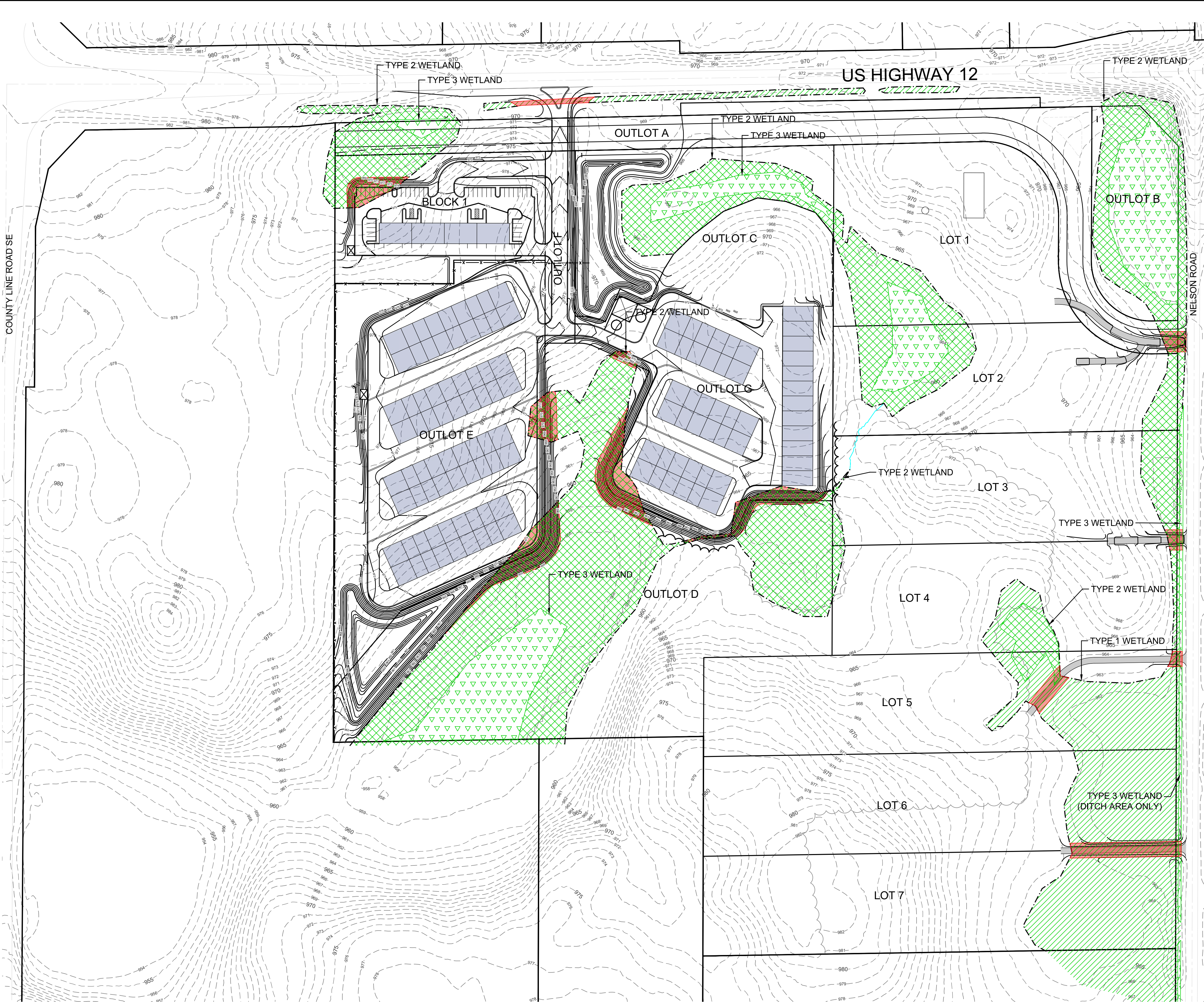
GRADING PLAN
SOUTH

DRAWING NO.

C402

PLOTTED:
02/10/2023

COMM. NO.
16915



LEGEND

—	PROPERTY LIMITS
- - - 966 - - -	EXISTING MINOR CONTOUR
- - - 965 - - -	EXISTING MAJOR CONTOUR
— 966 —	PROPOSED MINOR CONTOUR
— 965 —	PROPOSED MAJOR CONTOUR
[Green cross-hatch]	TYPE 1 WETLAND
[Green diagonal lines]	TYPE 2 WETLAND
[Green triangles]	TYPE 3 WETLAND
[Red hatched]	WETLAND IMPACT AREA

WETLAND AREAS

TYPE 1 = 185,968 SF. (4.27 AC.)
TYPE 2 = 377,554 SF. (8.67 AC.)
TYPE 3 = 119,507 SF. (2.74 AC.)
TOTAL = 638,029 SF. (15.68 AC.)

WETLAND IMPACT AREAS

TYPE 1 = 10,469 SF. (0.24 AC.)
TYPE 2 = 40,483 SF. (0.93 AC.)
TYPE 3 = 452 SF. (0.01 AC.)
TOTAL = 51,404 SF. (1.18 AC.)



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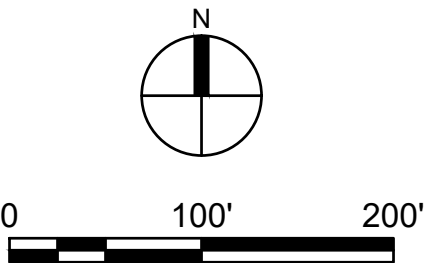
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WETLAND MITIGATION

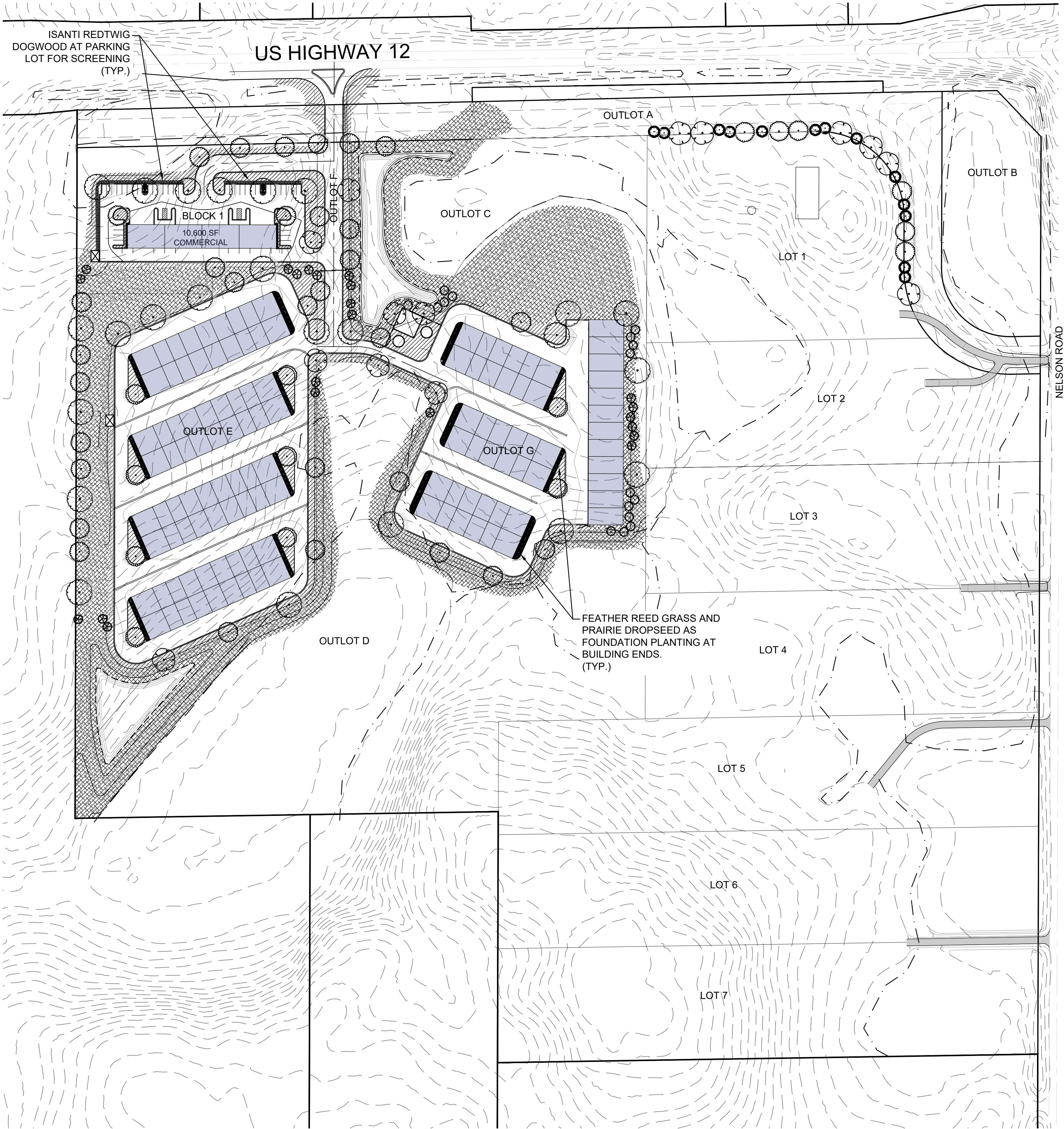
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C500

PLOTTED: 02/10/2023	COMM. NO. 16915
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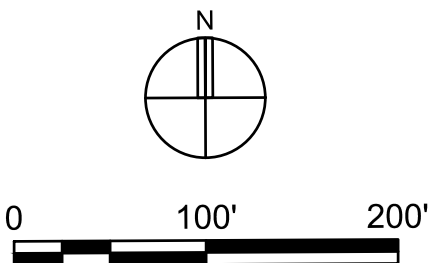
- PROPERTY LIMITS
- ADJACENT PROPERTY
- DELINEATED WETLAND
- NEW NATIVE SEED MIX MNDOT 33-261
- NEW TURF SEED MIX MNDOT 25-131
- 3" DEEP, SHREDDED HARDWOOD MULCH
- COMMERCIAL GRADE POLY EDGER
- DECIDUOUS OVERSTORY TREES
- CONIFEROUS TREES

NOTES

- REFER TO PLAN SHEET L2 FOR SODDING, SEEDING, FERTILIZER AND TOPSOIL NOTES
- ALL LANDSCAPING DISTURBED BEYOND THE NEW PLANTINGS SHALL BE REPLACED IN KIND.

PLANT SCHEDULE

TREES	QTY	COMMON / BOTANICAL NAME	CONT.	SIZE	REMARKS
	46	AUTUMN BLAZE® FREEMAN MAPLE ACER X FREEMANII 'JEFFERSRED'	B&B	2 - 1/2" CAL.	
	16	BLACK HILLS WHITE SPRUCE PICEA GLAUCA 'DENSATA'	B&B	6' HT.	
	22	AUSTRIAN PINE PINUS NIGRA	B&B	6' HT.	
	13	AUSTRIAN PINE PINUS NIGRA	B&B	8' HT.	
	14	SWAMP WHITE OAK QUERCUS BICOLOR	B&B	2 - 1/2" CAL.	
	24	JEFFERSON AMERICAN ELM ULMUS AMERICANA 'JEFFERSON'	B&B	2 - 1/2" CAL.	
SHRUBS	QTY	COMMON / BOTANICAL NAME	CONT.	SIZE	REMARKS
	82	ISANTI RED TWIG DOGWOOD CORNUS SERICEA 'ISANTI'	CONT.	5 GAL.	MIN. 5 CANES @ 12" HT.
PERENNIALS	QTY	COMMON / BOTANICAL NAME	CONT.	SIZE	REMARKS
	832	PRAIRIE DROPSEED SPOROBOLUS HETEROLEPIS	POT	3 GAL.	
GRASSES	QTY	COMMON / BOTANICAL NAME	CONT.	SIZE	REMARKS
	614	FEATHER REED GRASS CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	CONT.	5 GAL.	



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DRAWING TITLE

RESTORATION
PLAN

DRAWING NO.

L100

PLOTTED: 02/10/2023	COMM. NO. 16915
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GENERAL LANDSCAPE NOTES:

- LANDSCAPE CONTRACTOR SHALL VISIT THE PROJECT SITE PRIOR TO SUBMITTING A BID TO BECOME COMPLETELY FAMILIAR WITH SITE CONDITIONS.
- ALL ROUGH AND FINISH GRADING TO BE DONE BY OTHERS.
- NO PLANTING SHALL BE INSTALLED UNTIL ALL GRADING, BUILDING, CONSTRUCTION, UTILITY WORK & IRRIGATION (IF APPLICABLE) HAS BEEN COMPLETED IN THE AREAS TO BE PLANTED.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO IDENTIFY ALL UNDERGROUND CABLES, CONDUITS, WIRES, ETC., ON THE PROPERTY.
- IF THERE IS A DISCREPANCY BETWEEN THE NUMBER OF PLANTS SHOWN ON THE PLAN AND THE NUMBER OF PLANTS SHOWN IN THE PLANT LIST, THE NUMBER OF PLANTS SHOWN ON THE PLAN WILL TAKE PRECEDENCE.
- ALL PROPOSED PLANT MATERIAL SHALL BE LOCATED CAREFULLY AS SHOWN ON THE PLAN. IF THE CONTRACTOR BELIEVES AN ERROR HAS BEEN MADE REGARDING SPACING OR LOCATION OF THE PLANT MATERIAL INDICATED ON THE PLAN, NOTIFY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- THE CONTRACTOR IS RESPONSIBLE FOR COMPLETE MAINTENANCE OF THE PLANT MATERIAL (WATERING, SPRAYING, FERTILIZING, MOWING, ETC.) UNTIL THE WORK HAS BEEN ACCEPTED, BY THE OWNER.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL REPAIRS TO PROPERTY DAMAGE FROM PLANTING OPERATIONS AT NO COST TO THE OWNER.
- ALL NEWLY PLANTED PLANT MATERIAL SHALL BE GUARANTEED THROUGH ONE CALENDAR YEAR STARTING FROM THE DATE OF ACCEPTANCE ESTABLISHED BY THE OWNER.
- THE CONTRACTOR SHALL MEET WITH THE OWNER OR OWNERS REPRESENTATIVE ON SITE WHEN THEY FEEL THE PROJECT IS COMPLETE ACCORDING TO THE CONTRACT DOCUMENTS. IF ALL WORK IS SATISFACTORY AND COMPLETE ACCORDING TO THE CONDITIONS OF THE CONTRACT DOCUMENTS, THEN THE OWNER MUST DECLARE THE PROJECT COMPLETE. THIS DECLARATION WILL CONSTITUTE AS THE BEGINNING OF THE ONE (1) YEAR WARRANTY PERIOD FOR ALL PLANT MATERIAL. THE OWNER SHALL PROVIDE A LETTER WITH SIGNATURE STATING THE DATE OF ACCEPTANCE.
- WIND BURN OR OTHERWISE DAMAGED PLANT MATERIAL WILL NOT BE ACCEPTED.
- CONTRACTOR CAN SUBSTITUTE MACHINE MOVED MATERIAL USING APPROPRIATE SIZE TREE SPADE FOR B & B WITH OWNER APPROVAL.
- THE PRACTICE OF STAKING SHOULD NOT ALLOW NAILS, SCREWS, WIRES, ETC. TO PENETRATE THE OUTER SURFACE OF THE TREES.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR THE REMOVAL OF ALL TREE STAKES, GUYS, STRAPS AND TRUNK PROTECTION MEASURES FOLLOWING THE COMPLETION OF THE WARRANTY PERIOD OR AS DIRECTED BY THE OWNER.
- LANDSCAPE CONTRACTOR IS REQUIRED TO PROVIDE THE OWNER WITH MAINTENANCE INFORMATION DURING THE GUARANTEE PERIOD RELATING TO WATERING, FERTILIZING, PRUNING, PEST CONTROL, AND RELATED ITEMS. THIS WILL BE PREPARED AND DELIVERED TO THE OWNER AFTER PROVISIONAL INSPECTION APPROVAL HAS BEEN GIVEN BY THE OWNER AND/OR LANDSCAPE ARCHITECT.
- INSTALL CORRUGATED PLASTIC TREE GUARDS, WHITE IN COLOR, WITH THE SIZE OF TUBE 1" DIA. (MIN.) LARGER THAN THE CALIPER OF THE TREE TO BE PROTECTED.
- CONTRACTOR TO FURNISH & STALL PLASTIC EDGING AS SHOWN ON THE PLANS & DETAILS. PLASTIC EDGING SHALL BE MEDIUM DENSITY

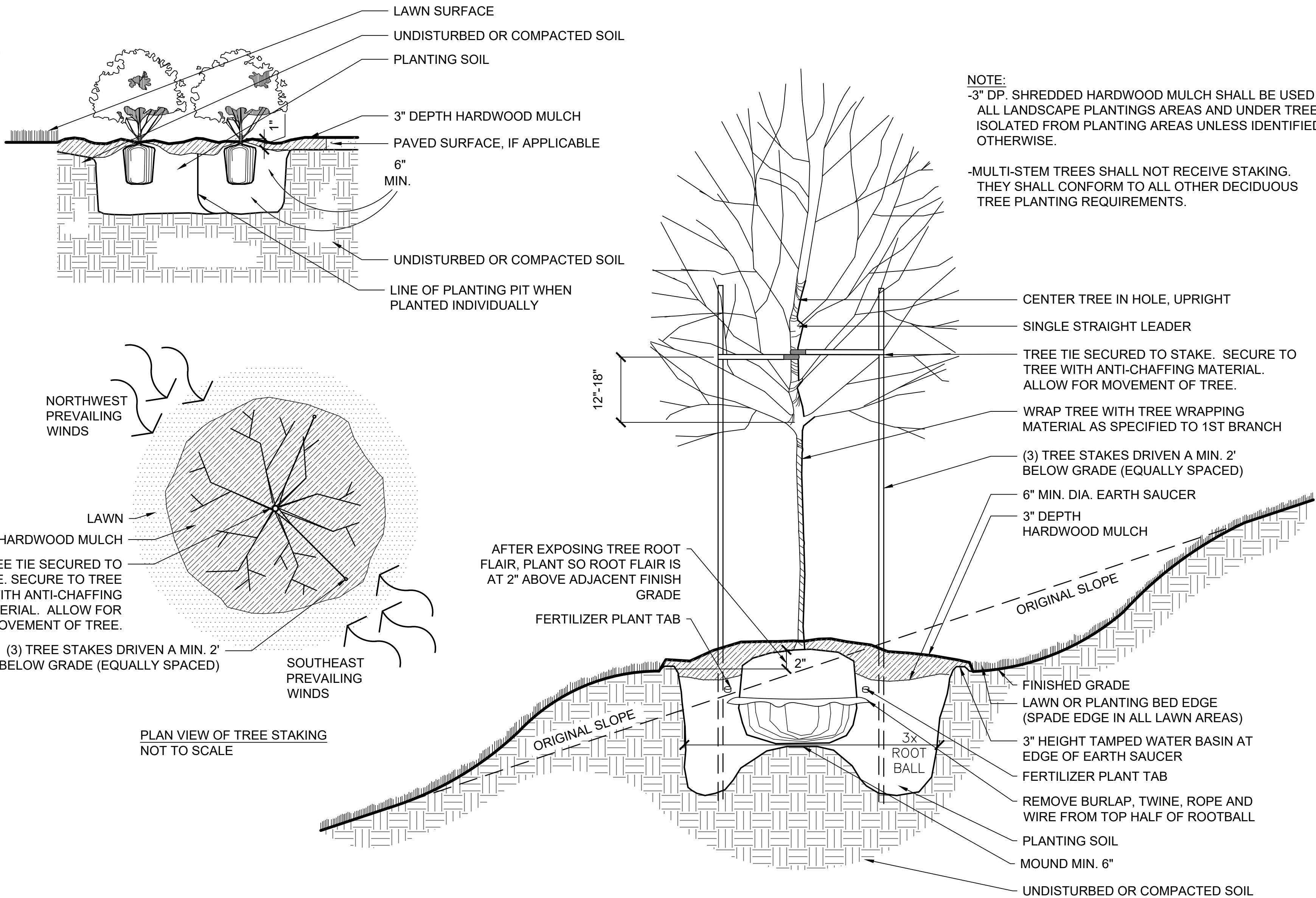
- POLYETHYLENE WITH U.V. INHIBITOR, BLACK IN COLOR, WITH A TOTAL DEPTH OF 5" (1" DIA. TOP AND 4" SHAFT WITH 1.5" V EVERY 3-1/2 FEET OF EDGING.
- LANDSCAPE FABRIC (FILTER MAT) TO HAVE A COMBINED WEIGHT OF 4.5-5.5 OZ. PER S.Y. FABRIC SHOULD BE U.V. STABILIZED AND HAVE A FIVE YEAR MINIMUM WEATHERABILITY FACTOR IN FULL SUNLIGHT. FABRIC TO BE PHILLIPS DUON R OR EQUIVALENT. SAMPLE REQUIRED FOR APPROVAL.
 - LANDSCAPE BED MULCH SHALL BE SHREDDDED HARDWOOD MULCH. MULCH SHALL BE INSTALLED AT A DEPTH OF 3" WITH NO WEED BARRIER LANDSCAPE FABRIC.
 - 3" DEPTH SHREDDDED HARDWOOD MULCH SHALL BE INSTALLED UNDER ALL TREES AND SHRUBS THAT ARE ISOLATED FROM GROUNDCOVER AREAS AND GENERAL SHRUB MASSES.
 - CALIPER OF TREES UP TO AND INCLUDING 4" SHALL BE MEASURED AT 6" ABOVE GROUND LEVEL, AND 12" ABOVE GROUND LEVEL FOR LARGER SIZES.
 - FOR BALLED & BURLAP PLANT MATERIAL, REMOVE THE TOP HALF OF THE BURLAP FROM THE ROOT BALL. WIRE CAGES, STRAPS, ETC. SHALL BE REMOVED FROM THE TOP HALF OF THE ROOTBALL BEFORE INSTALLATION.
 - ALL CONTAINER MATERIAL SHALL HAVE BEEN GROWN IN CONTAINER FOR A MINIMUM OF 6 MONTHS PRIOR TO INSTALLATION.
 - SHRUBS AND GROUNDCOVER SHALL BE PLANTED A MINIMUM OF ONE HALF THEIR ON-CENTER SPACING FROM PAVING EDGE UNLESS OTHERWISE NOTED.
 - DECIDUOUS SHRUBS SHALL HAVE MINIMUM OF FIVE (5) CANES AT SPECIFIED HEIGHT UNLESS OTHERWISE NOTED IN PLANT SCHEDULE.
 - LANDSCAPE CONTRACTOR SHALL PROVIDE AND INSTALL NURSERY GROWN PLANT MATERIAL CONFORMING TO THE REQUIREMENTS AND RECOMMENDATIONS OF THE LATEST EDITION OF ANSI Z60.1 STANDARDS UNLESS OTHERWISE NOTED IN THE PLANS OR SPECIFICATIONS.

GENERAL SODDING, SEEDING & TOPSOIL NOTES:

- LANDSCAPE CONTRACTOR SHALL ENSURE THAT NEW TREES MOVED ONTO THE SITE ARE DUG FROM SIMILAR SITES WITH SIMILAR SOILS TO THE SOILS OF THIS PROJECT (HEAVY TO HEAVY, LIGHT TO LIGHT. HEAVY TO LIGHT SOILS). CONTRACTOR SHALL REVIEW SOIL CONDITIONS/TYPES WITH OWNER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- ALL NEWLY INSTALLED PLANT MATERIAL SHALL BE PLANTED IN WELL-DRAINED AREAS. NOTIFY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION IF ANY PLANT MATERIAL IS LOCATED IN DRAINAGE SWALES OR WET & POORLY DRAINED AREAS.
- ALL PLANTINGS SHALL RECEIVE FERTILIZER AS FOLLOWS:
 - SUMMER AND FALL PLANTING: 0-20-20 GRANULAR (IN SAUCER AROUND PLANT AT THE RATE OF 12 OZ. PER 2-3" CAL. TREE & 6 OZ. PER SHRUB).
 - SPRING PLANTING: 10-10-10 GRANULAR (APPLY ABOVE REFERENCED FERTILIZER AT A RATE OF 12 OZ. PER 1-1/2" CAL. TREE OR LARGER & 6 OZ. PER SHRUB & PERENNIAL.
- ALL PLANTINGS SHALL RECEIVE AN AMENDED SOIL MIX CONSISTING OF THREE (3) PARTS:
 - 45% APPROVED TOPSOIL (ONE SITE PREFERRED)
 - 45% ORGANIC MATTER (TYPE 1 SPHAGNUM PEAT MOSS FINELY DIVIDED WITH A PH OF 3.1 - 5.0.)
 - 10% SAND (FINE CLEAN MASONRY SAND)
- AREAS CONFINED TO A MASS PLANTING AREA (PLANTING BED) SHALL RECEIVE THE AMENDED SOIL MIX AT MIN. 12" DEPTH THROUGHOUT THE PLANTING AREA. AMENDED SOIL MIX SHALL BE MIXED THOROUGHLY AND INSTALLED IN 6" LIFTS.
- AREAS TO RECEIVE SOD OR SEED SHALL HAVE A 6" MINIMUM DEPTH OF TOPSOIL. TOPSOIL SHALL PROVIDE FERTILE, FRIABLE, NATURAL LOAM, SURFACE SOIL, REASONABLY FREE OF SUBSOIL, CLAY CLUMPS, BRUSH WEEDS AND OTHER LITTER, AND FREE OF ROOTS, STUMPS, STONE LARGER THAN 1" IN ANY DIMENSION, AND OTHER EXTRANEIOUS OR TOXIC MATTER HARMFUL TO PLANT GROWTH.
- SOD SHALL BE HIGHLAND SOD, 30" X 100' ROLLS PREFERRED WHERE APPLICABLE, TO BE LAID PARALLEL WITH THE CONTOURS AND HAVE STAGGERED JOINTS. ON SLOPES STEEPER THAN 3:1 OR DRAINAGE SWALES, THE SOD SHALL BE STAKED INTO THE GROUND. SCARIFY THE EXISTING GRADES WITH FIELD CULTIVATOR PRIOR TO PLACING OF TOPSOIL AND FINISH GRADING FOR SOD. IMMEDIATELY PRIOR TO PLACING SOD, CONTRACTOR SHALL APPLY 10-6-4 FERTILIZER AT THE RATE OF 10 POUNDS PER 1000 SQ. FT.
- ALL AREAS SPECIFIED AS 'GENERAL LAWN SEED' TO BE SEEDED WITH PREMIUM SUNNY SEED MIX INCLUDING:
 - 20% RAVEN STAR KENTUCKY BLUEGRASS 10%

MIDNIGHT STAR KENTUCKY BLUEGRASS 15%
WASHINGTON KENTUCKY BLUEGRASS 5%
THERMAL BLUE BLUEGRASS 35%
SPLENDID PERENNIAL RYEGRASS 15%
ZODIAC CHEWINGS FESCUE(SEED MIX AVAILABLE AT GERTENS 1-651-450-1501). SEED SHALL BE APPLIED AT A RATE OF 150 LBS./AC. WITH 2 TONS SHREDDDED AND PUNCHED IN PLACE STRAW PER ACRE. HYDROSEEDING & MULCHING IS AND ACCEPTABLE ALTERNATE.

- ALL AREAS SPECIFIED AS 'ROUGH SEED' TO BE SEEDED WITH MNDOT SEED MIX 250 AT A RATE OF 150 LBS./AC. WITH 2 TONS SHREDDDED AND PUNCHED IN PLACE STRAW PER ACRE. HYDROSEEDING & MULCHING IS AN ACCEPTABLE ALTERNATE.
- ALL AREAS SPECIFIED AS 'WET AREA SEED' TO BE SEEDED WITH MNDOT MIX 310. BROAD CAST, DRILL AND/OR HYDROSEED AT A RATE OF 150 LBS./AC. WITH 2 TONS SHREDDDED AND PUNCHED IN PLACE STRAW PER ACRE. SEED MIX SHALL BE ABLE TO WITHSTAND PERIODIC FLOODING.
- ALL AREAS SPECIFIED AS "MNDOT 25-131" TO BE SEEDED PER MNDOT RECOMMENDED APPLICATION RATES AND METHODS. "MNDOT 25-131" OR APPROVED EQUAL IS A BASIS OF DESIGN. CONTRACTOR IS RESPONSIBLE FOR ALL EROSION CONTROL NEEDS TO ESTABLISH VIGOROUS GROWTH PER MNDOT 2014 SEEDING MANUAL STANDARDS.



NOTE:
-3" DP. SHREDDDED HARDWOOD MULCH SHALL BE USED IN ALL LANDSCAPE PLANTINGS AREAS AND UNDER TREES ISOLATED FROM PLANTING AREAS UNLESS IDENTIFIED OTHERWISE.

-MULTI-STEM TREES SHALL NOT RECEIVE STAKING. THEY SHALL CONFORM TO ALL OTHER DECIDUOUS TREE PLANTING REQUIREMENTS.



ANDERSON
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P 763.412.4000 | F 763.412.4090
Anderson Engineering of Minnesota, LLC

MIXED USE DEVELOPMENT

INDEPENDENCE, MINNESOTA

STODDARD COMPANIES

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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DRAWING TITLE

LANDSCAPE DETAILS

DRAWING NO.

L101

PLOTTED: 02/10/2023	COMM. NO. 16915
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This architectural rendering depicts a modern industrial or commercial building complex. The primary building on the left is a two-story structure with a dark, flat roof and a facade of vertical metal slats. It features large glass windows and a glass-enclosed upper level where a person is visible. The building is numbered '1101' and '1102' above its large glass doors. A silver sports car is parked in the foreground. To the right, another similar building is partially visible. The scene is set in a paved parking lot with small landscaped islands containing shrubs. The sky is bright blue with scattered white clouds.

OWNER/DEVELOPER: STODDARD COMPANIES
925 EXCELSIOR BLVD
EXCELSIOR, MN 55331
Ph: 612-802-3666

ARCHITECT: ELNESS SWENSON GRAHAM ARCHITECTS INC.
500 WASHINGTON AVE. SOUTH, SUITE 1080
MINNEAPOLIS, MN 55415
Ph: 612-339-5508
Fx: 612-339-5382

GENERAL CONTRACTOR: TBD

		07/01/23
DRAWING NUMBER	DRAWING NAME	
ARCHITECTURAL		
A0.1	TITLE SHEET	●
A1.1	FLOOR PLAN - LEVEL 1	●
A1.2	FLOOR PLAN - MEZZANINE	●
A1.3	ROOF PLAN	●
A3.1	BUILDING ELEVATIONS - MOTOR CONDOS	●
A3.2	BUILDING ELEVATIONS - COMMERCIAL	●
A3.3	EXTERIOR RENDERINGS - MOTOR CONDOS	●
A3.4	EXTERIOR RENDERINGS - COMMERCIAL	●

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[illegible]

FWHS	FLAT HEAD WINDOW SCREW	HNDRL	HANDRIAL
FWN	WINDOW FIN	HORIZ	HORIZONTAL
FX	FINISH	HJ	HOUR
FX1	FIXTURE	H5	H5 STRENGTH
FL	FLOW LINE	HT	HEIGHT
FLDG	FOLDING	HTR	HEATER
FLX	FLEXIBLE	HVAC	HEATING, VENTILATING, AIR CONDITIONING
FLC	FLUORESCENCE	HW	HOT WATER
FLR	FLOOR (ING)	HYD	HYDRANT
FLU	FLUORESCENT		
FN	FENCE	I	INCH
FOC	FACE OF CONCRETE	ID	INSIDE DIAMETER
FOE	FACE OF EGG	IE	INVERT ELEVATION
FOM	FACE OF MASONRY	IN	INCH (ES)
FOS	FACE OF SLABTING	INCAND	INCANDESCENT
FP	FLIRFPAKE	INCR	INCREASER
FPF	PIREPROOF (ING)	INCL	INCLUDE (D) (ING)
FB	FRAMER	INCO	INFORMATION
FBR	FIBERGLASS REINFORCED PLASTIC	INCR	INSULAT (E) (ION)
FS	FOOT SIZE	INCL PNL	INCLUDED PANEL
FSE	FIELD SERVICE EQUIPMENT CONTRACTOR	INT	INTERIOR
FSF	FIELD STANDPIPE	INTER	INTERLUATED AREA
FSDN	FASTENER		
FSSTOP	FIELD STOPPING	J	JOINT
FT	FOOT & FEET	JB	JUNCTION BOX
FTD	FOOTING	JC	JANITOR'S CLOSET
FTF	FEET TON ADAPTATION	JST	JOIST
FUR	FURB (ING)	JT	JOINT
FURN	FURNACE	JTBR	JOINT BRACKET
FUT	FUTURE		
PV	FIELD VERIFY	K	KNOCK
FWC	FABRIC WALL COVERING	KD	KNOCK DOWN
		KH	KITCHEN
G	GAGE	KO	KNOCKOUT
GAUV	GALVANIZED	KOP	KNOCKOUT PANEL
GAR	GARAGE	KPS	KNOCK-PAKE
GR	GRAB BAR		
GE	GENERAL CONTRACTOR	L	LEAF
GEN	GENERATOR	L1	LEFT
GENL	GENERAL	LAB	LABORATORY
GI	GALVANIZED IRON	LAV	LAVATORY
GL	GLASS	LBN	LOADING
GLU	GLUE LAMINATED	LBL	LABEL
GLZ	GLAZING	LCMU	LEAD CEMENT MORTAR CONCRETE MASONRY UNIT
GLZM	GLAZING MORTAR CONCRETE MASONRY UNIT	LD	LOADING
GR	GRADE	LF	LINEAL FOOT (FEET)
GRG	GRATING	LFH	LENGTH & HEAD
GS	GRASS SHIELD	LFH	LENGTH & HEAD
GT	GROUT	LHR	LEFT HAND REVERSE
GV	GRAVITY VENT	LIB	LIBRARY
GWT	GALVANIZED WALTILE	LINO	LINOLEUM
GYP	GYPSON	LDR	LOCKER
		LL	LONG LEAD
H	HIGH	LWH	LONG HIGH HORIZONTAL
HB	HOLLOW BIBB	LWL	LONG LOW HORIZONTAL
HC	HOLLOW CORE	LUNL	LUNEL
HCB	HOLLOW CORE REINFORC	LR	LYING ROOM
HW	HOLLOW CORE WOOD	LSE	LARGE
		LSD	LIQUID SOAP DISPENSER
HD	HARD BOARD	LT	LIGHT
HDOP	HANDICAP	LT WT	LIGHTWEIGHT
HDS	HESAR	LVL	LAMINATED VENER LUMBER
HDWD	HARDWOOD	LVR	LOVER
HONE	HARDWARE		
HEX	HEXAGON (AL)	MAINT	MAINTENANCE
HML	HOLLOW METAL	MAS	MASONRY
HMED	HOLLOW METAL KNOCK DOWN	MAST	MATERIAL

MAX	MAXIMUM
MAR	MARBLE
MBR	MASTER BEDROOM
MBR	MEMBER
MCC	METAL MECHANICAL
MED CAB	MEDICINE CABINET
MEMB	MEMBRANE
MET	METAL
MEZZ	MEZZANINE
MFG	MANUFACTURING
MFR	MANUFACTURER
MHI	MANHOLE
MISO	MISDO
MILWAK	MILLOWALK
MIM	MIMIMUM
MIR	MIRROR
MISC	MISCELLANEOUS
MR BOARD	MIRROR BOARD
MOLD	MOLDING
MOLDG	MOLDING
MOT	MACHINE OPENING
MOT	MODULE (AL)
MS	MAN'S TOILET
MTG	MOUNTED
MTG	MOUNTING
MTR	MOTOR
MULL	MULLION
MULT	MULTIPLE
MVBL	MULTIPLE
N	NORTH
NA	NOT APPLICABLE
NIC	NURSE CALL
NIC	NOT IN CONTRACT
NO	NUMBER
NOM	NOMINAL
NOS	NONE
NRS	NOISE REDUCTION COEFFICIENT
NTS	NOT TO SCALE
O.C.	ON CENTER
O.D.	OUTSIDE DIAMETER
OA	OVERALL
OBS	OBSCURE
OC	OCTAGON (AL)
OFF	OFFICE
OH	OVERHEAD
OND	OVERSIZED
OP	OPERABLE PARTITION
OPNG	OPENING
OPF	OPPOSITE
ORF	OVERFLOW/LOW FLOOR
ORV	ORVAIN
OS	OUTSIDE
P	PUSH BUTTON
PBD	PARALLEL BOARD
PBL	PULBING
PC	PRICE
PCG	PRECAST CONCRETE
PCD	PAPER CUP DISPENSER
PCF	POUNDS PER CUBIC FOOT
PER	PERISTALTIC
PERF	PERFORATED
PERM	PERMETER
PERP	PERPENDICULAR
PL	PLATE
PL	PROPERTY LINE

PLAM	PLASTIC LAMINATE
PLAS	PLASTER
PLYWD	PLYWOOD
PNL	PANEL
PNT	PINT
POL	POLISHED
PP	POWER POLE
PPR	POUR PRESERVATIVE TREATED
PR	PAIR
PREFAB	PRE-FABRICATED
PREFIN	PRE-FINISHED
PREP	PREFARE
PROJ	PROJECT
PRV	POWER ROOF VENTILATOR
PS	POLISHED SQUARE FOOT
PSQ	POLISH SQUARE FEET
PTS	POINT TENSORED (L&B BEAM)
PU	PAVER TILES UNFINISHED
PTN	PARTITION
PTP	PAVER TILES RECTANGLE
PUCHL	PUWER CHLORE
PVMT	PAVEMENT
Q	
QT	QUARRY TILE
QTR	QUARTER
QTY	QUANTITY
R	
R	ROSE
R & S	ROOF AND SHELF
R&B	RETURN AIR
RAD	RADIUS
RBS	RESILIENT BASE
RBR	RUBBER
RC	REINFORCED CHANNEL
REC	RECTIFIED CEILING PLAN
RD	ROAD SHOULDER
REC	RECTIFIED (ING)
RECPT	RECEIPT
REC	RECTANGLE (L&B) (L&R)
REF	REFER
REF	REFRIGERATOR
REIN	REINFORCING (ING)
REC	REQUIRED
REIN	REIN. REIN. PLAN
RESIL	RESILIENT
RET	RETURN
REV	REVERSE
RF	RESILIENT FLOOR (ING)
ROD	RODGING
RH	RIGHT HAND
RMC	RIGHT HAND MERVINE
RMD	RIGHT HAND
RNC	RECTESSE MEDICINE CABINET
RO	ROOM
RNG	RANGE
ROD	ROD DRESSING
RB	RAILROAD
RWL	RAINWATER LEADER
S	
S	SOUTH
SA	SUPPLY AIR
SB	SIDE BAR
SC	SOLID CORE
SCHD	SOLID CORE HARD BOARD
SCHS	SCHEDULE
SCHS	SEALER-CORNER, STAINED CORNER

SCRN	SHOWER CURTAIN ROD
SCREEN	SCREEN
SCU	SQUARE CORNER UNIT
SCW	SOUL-CORE WOOD
SD	SOIL DRY
SDA	SELF DRAINING AGGREGATE
SDG	SIDING
SDSP	SOAP DISPENSER
SECT	SECTION
SEF	SQUARE FOOT (FEET)
SF	SPRAY-ON FREEFRODING
SH	SHEET (VES) (VINGS)
SHI	SHOWER HEAD
SHIT	SHEET
SHY	SHINING
SIMT	SQUARE INCH (ES)
SIM	SIMILAR
SK	SKIM
SLN	SEALANT
SLP	SLOPE
SLV	SLIVER
SND	SANITARY NARIN DISPENSER
SNDP	SANITARY NARIN RECEPTACLE
SP	STANDPIPE
SPEC	SPECIFICATION
SPKR	SPEAKER
SPKR	SPEAKER
SQL	SQUARE
SS	SANITARY SEWER
SSF	SOUL SURFACE (FRSH)
SSD	SEWER DRY
ST	STAINLESS STEEL
ST	SEAMLESS SHEET VING
ST	STEEL
ST	STAIN
ST	STREET
ST	STANDARD
STN	STONE
STOR	STORAGE
STR	STRUT
STRUC	STRUCTURAL
STRUP	SUPPLIER
STRUP	SUSPENDED
SV	SHEET VING
SVB	SEMI VINYL BASE
SWAR	SWAR
SWR	SWIRL
SWR	SWIRL HARD ISS
SYN	SYNTHETICAL
YS	SYSTEM
Y	
Z	
T	TREAD
T&B	TOWEL AND BOTTOM
T&B	TONGUE AND GROOVE
T.O.	T.O.P.
TB	TOWEL BAR
TC	TRAFFIC COATING
TD	TOWEL DISPENSER
TD	TOWEL DISPENSER
TD	TRENCH ANOD
TDRP	TOWEL DISPENSER
TDR	TOWEL DISPENSER/RECEPTACLE
TEL	TELEPHONE
TEMP	TEMPERATURE
TER	TERMINATION (ATE)
TER	TERMINATED
TH	THRESHOLD
THK	THICK (NESS)
THRU	THROUGH

TS	TACABORD
TS	TACSHIP
TS	TEMPERED
TO	TOILET
TOIL	TOILET
TOIP	TOILET PARTITION
TOIP	TOILET PAPER DISPENSER
TR	TRAFIC
TRD	TRAFIC DOOR
TR	TRANS
TRD	TREATED
TR	TRUCK SECTION
TV	TELEVISION
TV	TYPICAL
U	
U	UNDER SIDE
U	UNIT HEATER
UNEK	UNECAUTED
UNEN	UNENHANCED
UNEN	UNLESS NOTED OTHERWISE
UR	URINAL
UTL	UTILITY
V	
VAC	VACUUM
V	VINYL BASE
VCP	VITRIFIED CLAY PIPE
VCT	VINYL COMPOSITE FLEE
V	VENDING
V	VENTILATE (IDE) (OR)
V	VERTICAL
V	VEST
V	VESSEL
VIN	VINYL
V	VISOR
V	VAPOR RETARDER
VTR	VENT THROUGH ROOF
VW	VINYL WALL COVERING
W	
W	WASTE
W	WEST
W	WIDE WIDTH
W/	WITH
W	WITHIN
W/D	WITHOUT
W	WALL TO WALL
WC	WATER CLOSET
W	WOOD
W	WOOD BASE
WOSP	WASTE DISPOSER
W	WINDOW
W	WINE GLASS
WH	WALL HUNG
WH	WALL HYPRANT
WH	WATER HEATER
WH	WROUGHT IRON
WH	WATERPROOF (ING)
WP	WORKING POINT
W	WATER RESISTANT
WS	WEATHERSTRIP (ING)
W	WANG
W	WOMEN'S TOILET
W	WELDED WIRE FABRIC
X	
X	BY (45 X 45)
Y	
YD	YARD

	SECTION OR EXTERIOR ELEVATION		EXTERIOR ELEVATION KEYNOTE
	INTERIOR ELEVATION		ROOM NAME AND NUMBER
	DETAIL OR ENLARGED PLAN		DOOR NUMBER
	WALL TYPE		SECTION KEYNOTE
	WINDOW TYPE		REVISION NUMBER
	ELEVATION		FIRE EXTINGUISHER CABINET
	SPOT ELEVATION		FIRE EXTINGUISHER - WALL MOUNTED
	FLOOR FINISH TRANSITION		MOUNTING HEIGHTS DIAGRAM KEYNOTE

A0.1

Motor
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Independence, MN

esg

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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota

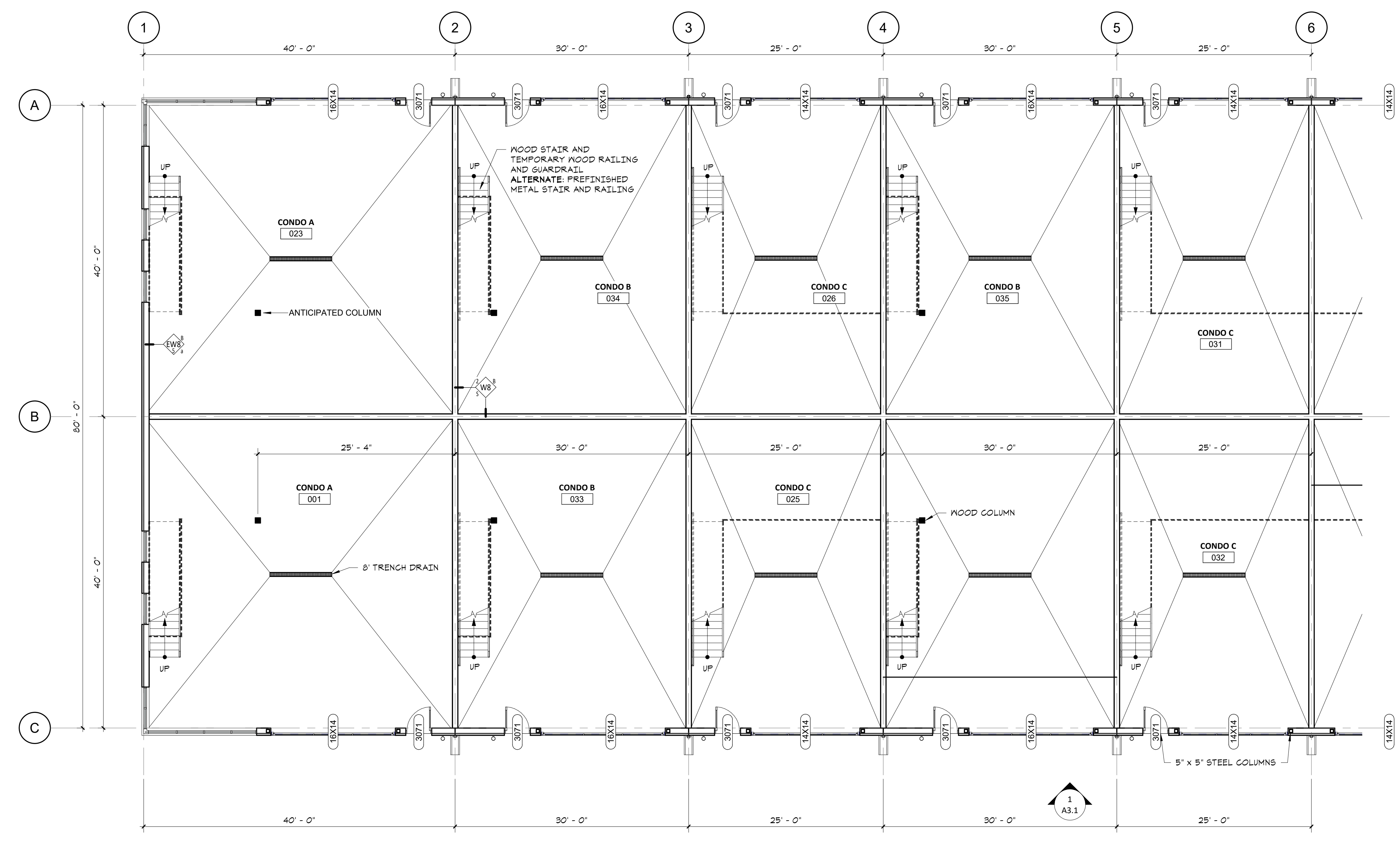
Signature

Typed or Printed Name

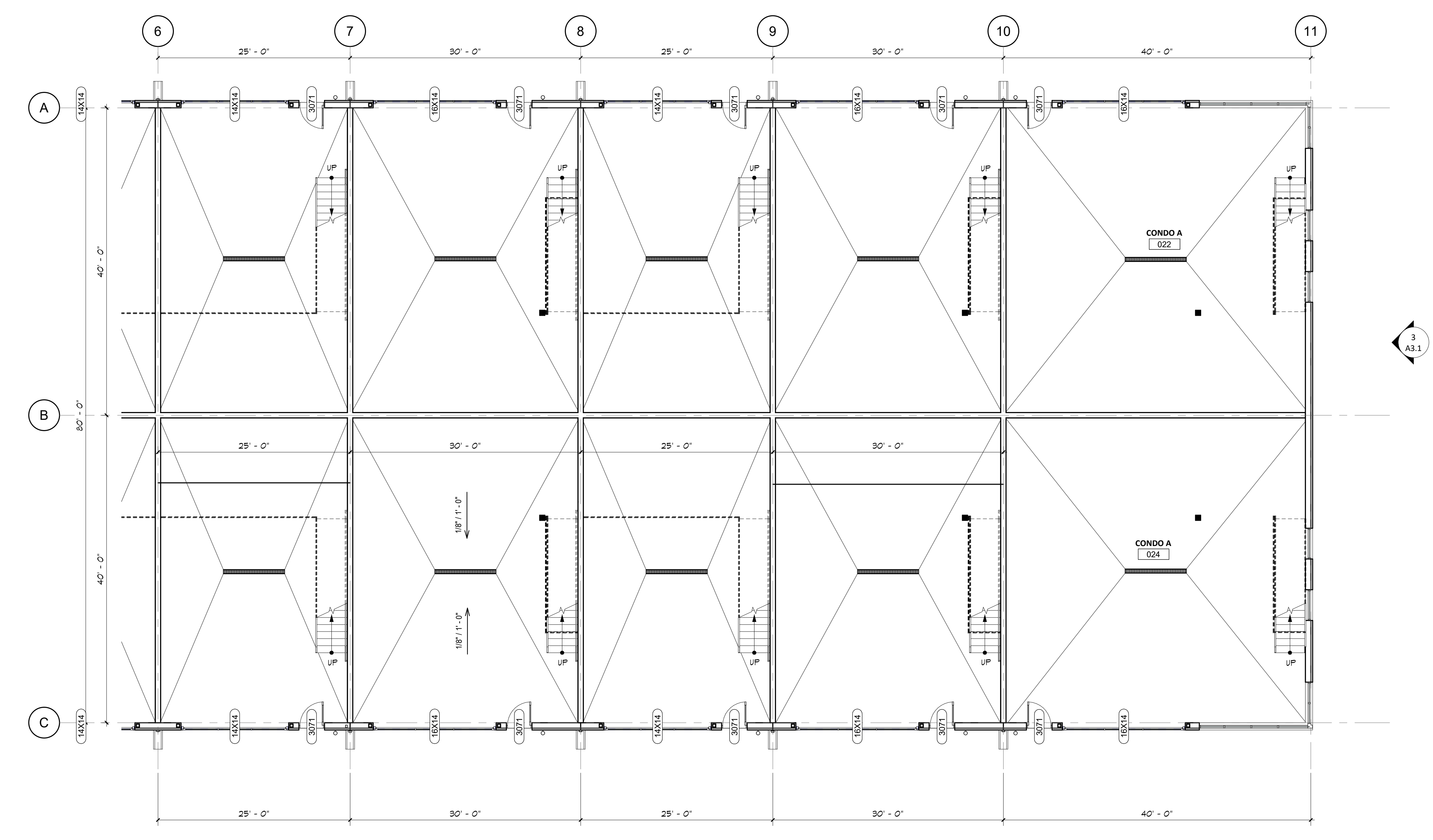
License # Date

NOT FOR
CONSTRUCTION

- FLOOR PLAN GENERAL NOTES
- DIMENSIONING STANDARDS:
 - A. MASONRY WALL DIMENSIONS ARE TO FACE OF MASONRY.
 - B. INTERIOR STUD WALL DIMENSIONS ARE TO CENTERLINE OF STUD.
 - EXTERIOR WALL DIMENSIONS ARE TO FACE OF SHEATHING.
 - PROVIDE FIRE EXTINGUISHER CABINETS WHERE INDICATED ON PLANS- MATCH BUILDING STANDARD.
 - PROVIDE WOOD OR SHEET METAL BLOCKING FOR ALL EQUIPMENT MOUNTED IN GYP BOARD PARTITIONS. MAINTAIN INTEGRITY OF FIRE AND/OR SMOKE RATING WHERE ACCESSORIES ARE IN RATED WALLS.
 - PROVIDE DIAGONAL BRACING TO SLAB AT DOUBLE STUDS AT DOOR OPENINGS AND AT DROP ARCHES/BULKHEADS AT 60" O.C. MAX.
 - ALL GLASS SHALL BE CLEAR TEMPERED SAFETY GLASS.
 - PROVIDE FLAMMABLE WASTE TRAP SYSTEM TO SERVE ALL GARAGE DRAINS.



2 LEVEL 1 FLOOR PLAN - WEST
A1.1 1/8" = 1'-0"



1 LEVEL 1 FLOOR PLAN - EAST
A1.1 1/8" = 1'-0"

SCHEMATIC
DESIGN

ORIGINAL ISSUE:
02/10/23

REVISIONS:
No. Description Date

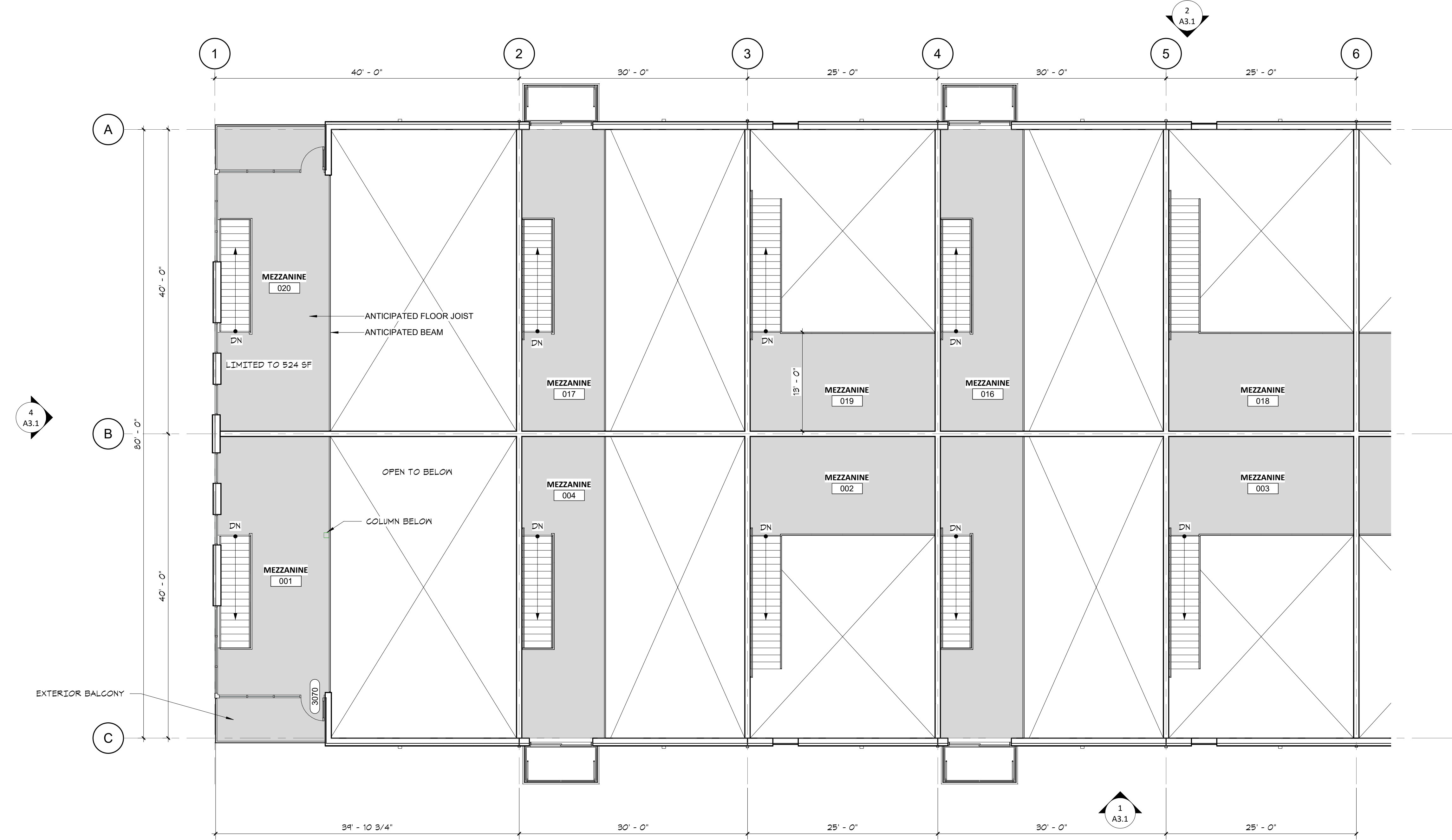
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PROJECT NUMBER

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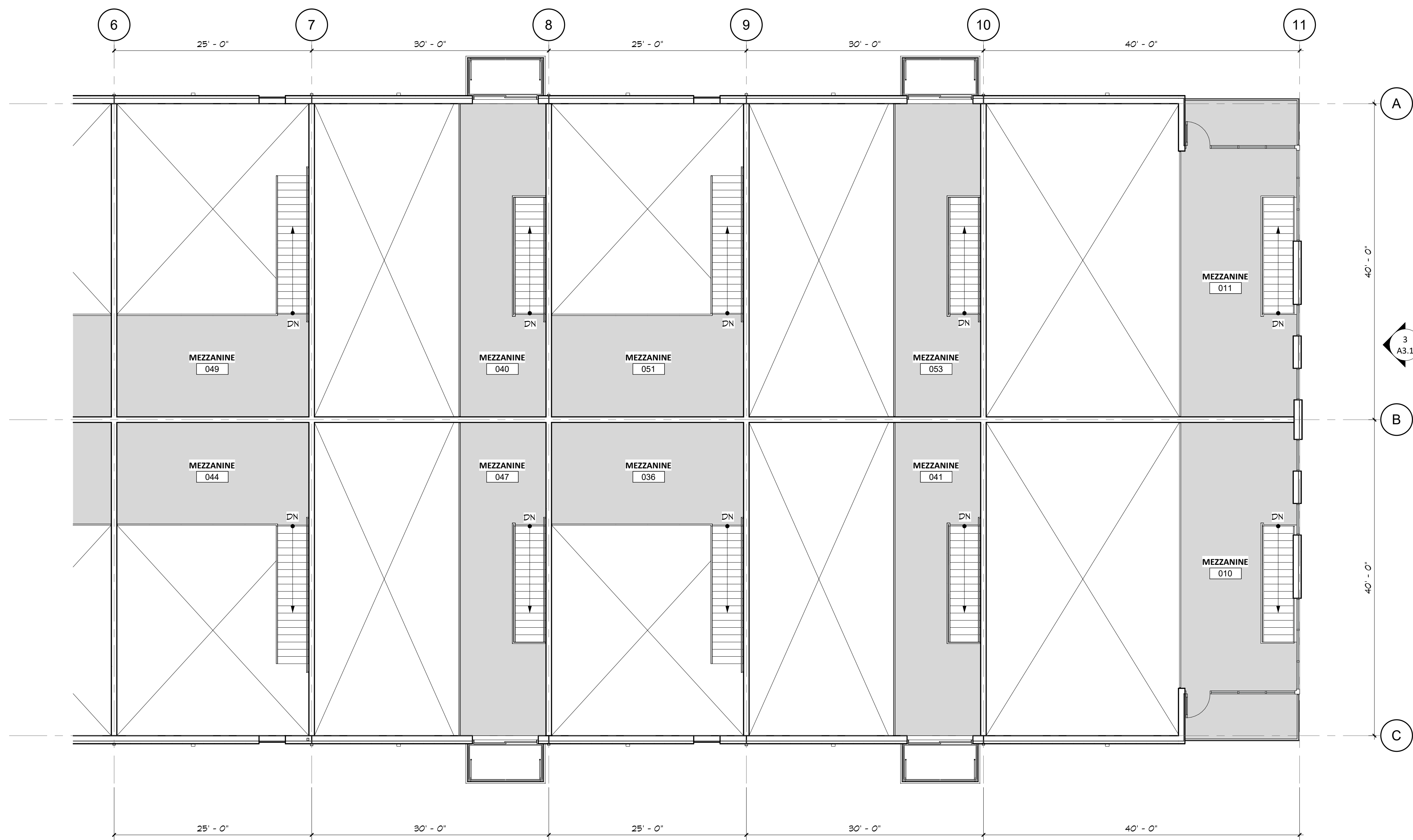
KEY PLAN

FLOOR PLAN - LEVEL 1

A1.1



2 MEZZANINE FLOOR PLAN - WEST
A1.2 1/8" = 1'-0"



1 MEZZANINE FLOOR PLAN - EAST
A1.2 1/8" = 1'-0"

FLOOR PLAN GENERAL NOTES

- DIMENSIONING STANDARDS:
 - A. MASONRY WALL DIMENSIONS ARE TO FACE OF MASONRY.
 - B. INTERIOR STUD WALL DIMENSIONS ARE TO CENTERLINE OF STUD.
- EXTERIOR WALL DIMENSIONS ARE TO FACE OF SHEATHING.
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- ALL GLASS SHALL BE CLEAR TEMPERED SAFETY GLASS.
- PROVIDE FLAMMABLE WASTE TRAP SYSTEM TO SERVE ALL GARAGE DRAINS.

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I hereby certify that this plan, specification, or
report was prepared by me or under my direct
supervision and that I am a duly licensed architect
under the laws of the State of Minnesota

Signature

Typed or Printed Name

License #

Date

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ORIGINAL ISSUE:
02/10/23

REVISIONS:
No. Description Date

221716
PROJECT NUMBER

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KEY PLAN

FLOOR PLAN -
MEZZANINE

A1.2

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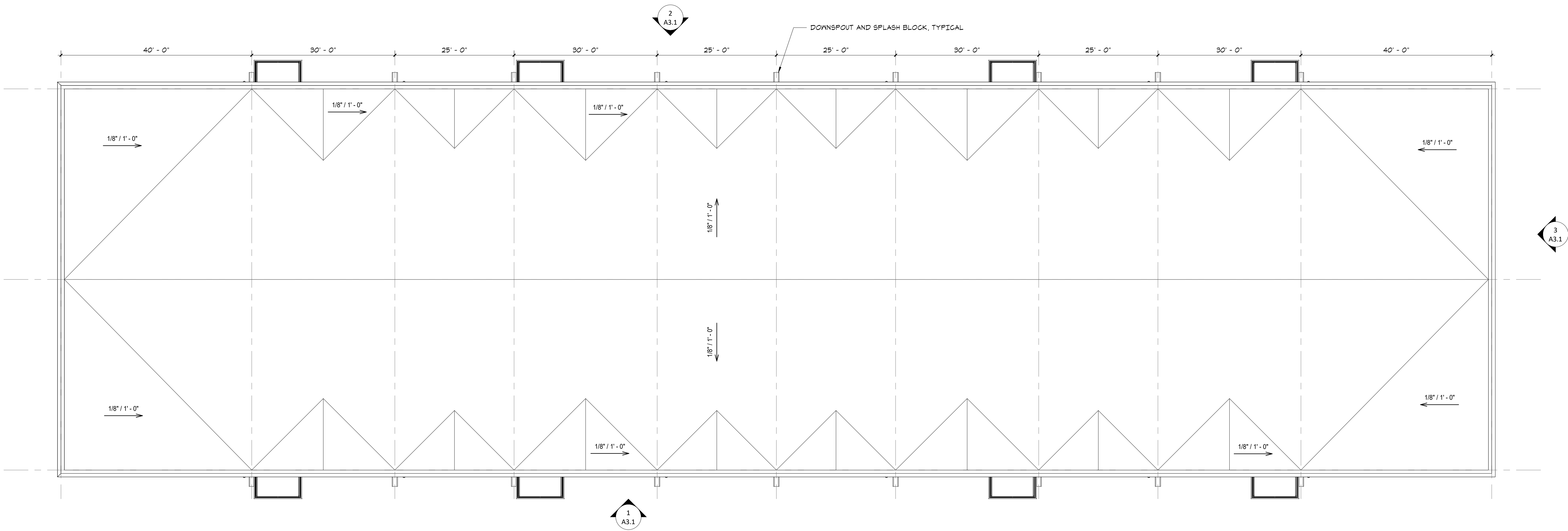
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Date

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1 ROOF PLAN
A1.3 3/32" = 1'-0"

SCHEMATIC
DESIGN

ORIGINAL ISSUE:
02/10/23

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KEY PLAN

ROOF PLAN

A1.3

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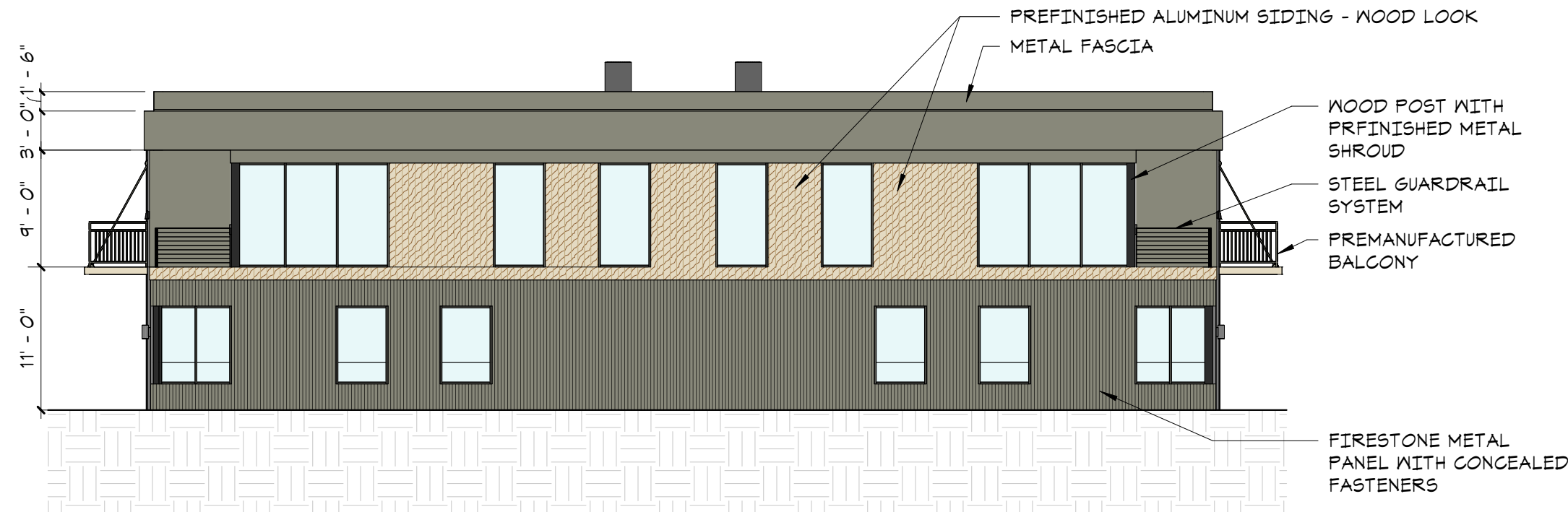
Signature

Typed or Printed Name

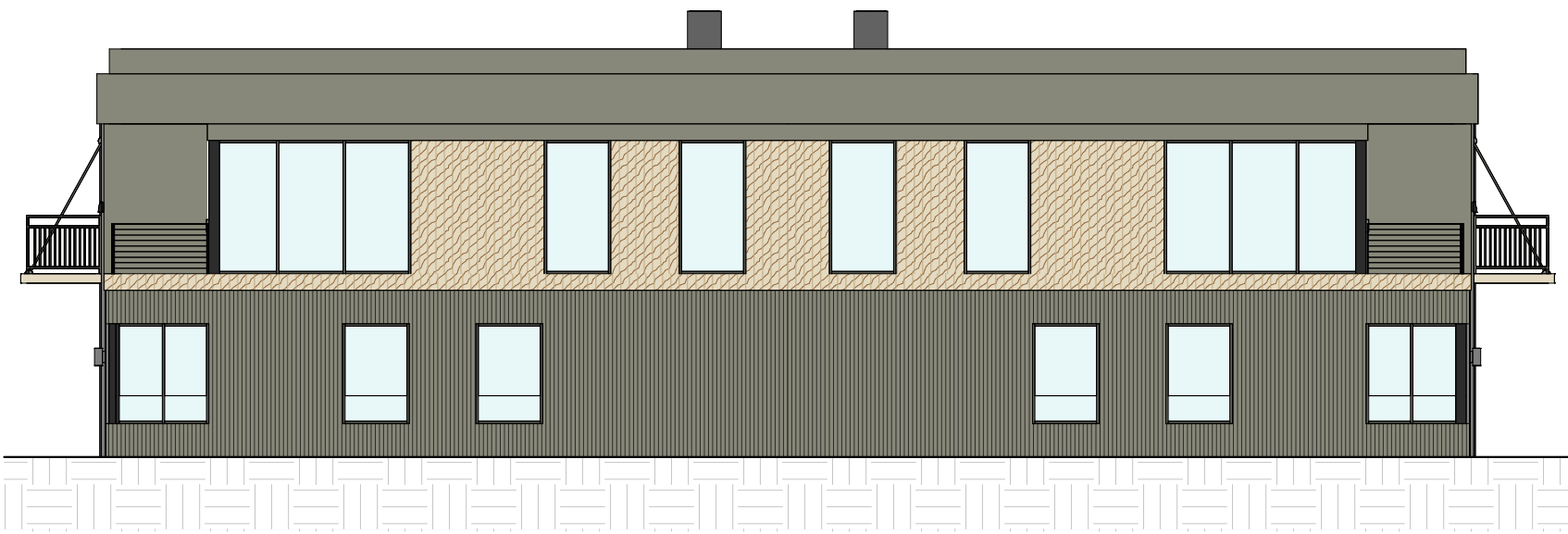
License #

Date

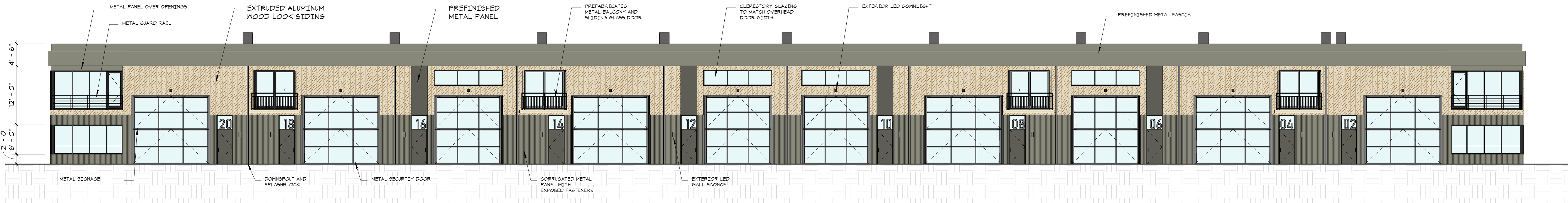
NOT FOR
CONSTRUCTION



4 MOTOR CONDOS - SIDE ELEVATION
A3.1 3/32" = 1'-0"



3 MOTOR CONDOS - SIDE ELEVATION
A3.1 3/32" = 1'-0"



2 MOTOR CONDOS - BACK ELEVATION
A3.1 3/32" = 1'-0"



1 MOTOR CONDOS - FRONT ELEVATION
A3.1 3/32" = 1'-0"

SCHEMATIC
DESIGN

ORIGINAL ISSUE:
02/10/23

REVISIONS:
No. Description Date

221716
PROJECT NUMBER

CKB ESG
DRAWN BY CHECKED BY

KEY PLAN

BUILDING ELEVATIONS -
MOTOR CONDOS

A3.1

Motor
Condos at
Independence

9285 US Highway 12
Independence, MN

esg

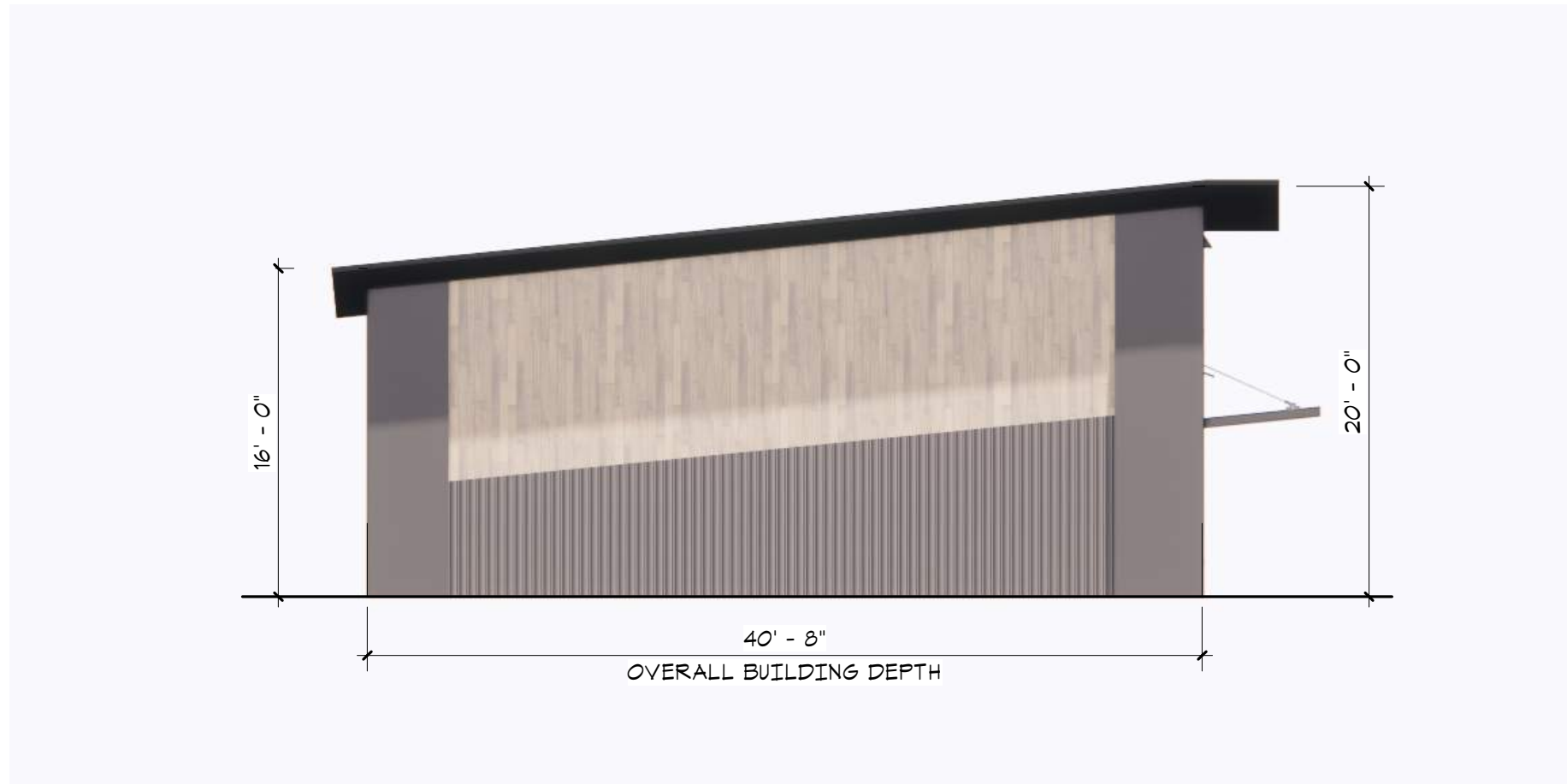
ARCHITECTURE & DESIGN

500 Washington Avenue South, Suite 1080
Minneapolis, MN 55415
p 612.339.5508 | f 612.339.5382
www.esgarch.com

I hereby certify that this plan, specification, or
report was prepared by me or under my direct
supervision and that I am a duly licensed architect
under the laws of the State of Minnesota

Signature
Typed or Printed Name
License # Date

NOT FOR
CONSTRUCTION



2
A3.2
COMMERCIAL BUILDING - SIDE ELEVATION
1/8" = 1'-0"



1
A3.2
COMMERCIAL BUILDING - FRONT ELEVATION
1/8" = 1'-0"

SCHEMATIC
DESIGN

ORIGINAL ISSUE:
02/10/23

REVISIONS:
No. Description Date

221716
PROJECT NUMBER

JZ
DRAWN BY

ESG
CHECKED BY

KEY PLAN

BUILDING ELEVATIONS -
COMMERCIAL

A3.2

Motor
Condos at
Independence

9285 US Highway 12
Independence, MN



500 Washington Avenue South, Suite 1080
Minneapolis, MN 55415
p 612.339.5508 | f 612.339.5382
www.esgarch.com

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Typed or Printed Name
License # Date

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CONSTRUCTION



EXTERIOR RENDERING - MOTOR CONDOS

SCHEMATIC
DESIGN

ORIGINAL ISSUE:
02/10/23
REVISIONS:
No. Description Date

221716
PROJECT NUMBER
CKB ESG
DRAWN BY CHECKED BY
KEY PLAN

EXTERIOR RENDERING -
MOTOR CONDOS

A3.3

Motor
Condos at
Independence

9285 US Highway 12
Independence, MN



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www.esgarch.com

I hereby certify that this plan, specification, or
report was prepared by me or under my direct
supervision and that I am a duly licensed architect
under the laws of the State of Minnesota

Signature _____
Typed or Printed Name _____
License # _____ Date _____

NOT FOR
CONSTRUCTION



AERIAL - COMMERCIAL BUILDING



PERSPECTIVE - COMMERCIAL BUILDING

SCHEMATIC
DESIGN

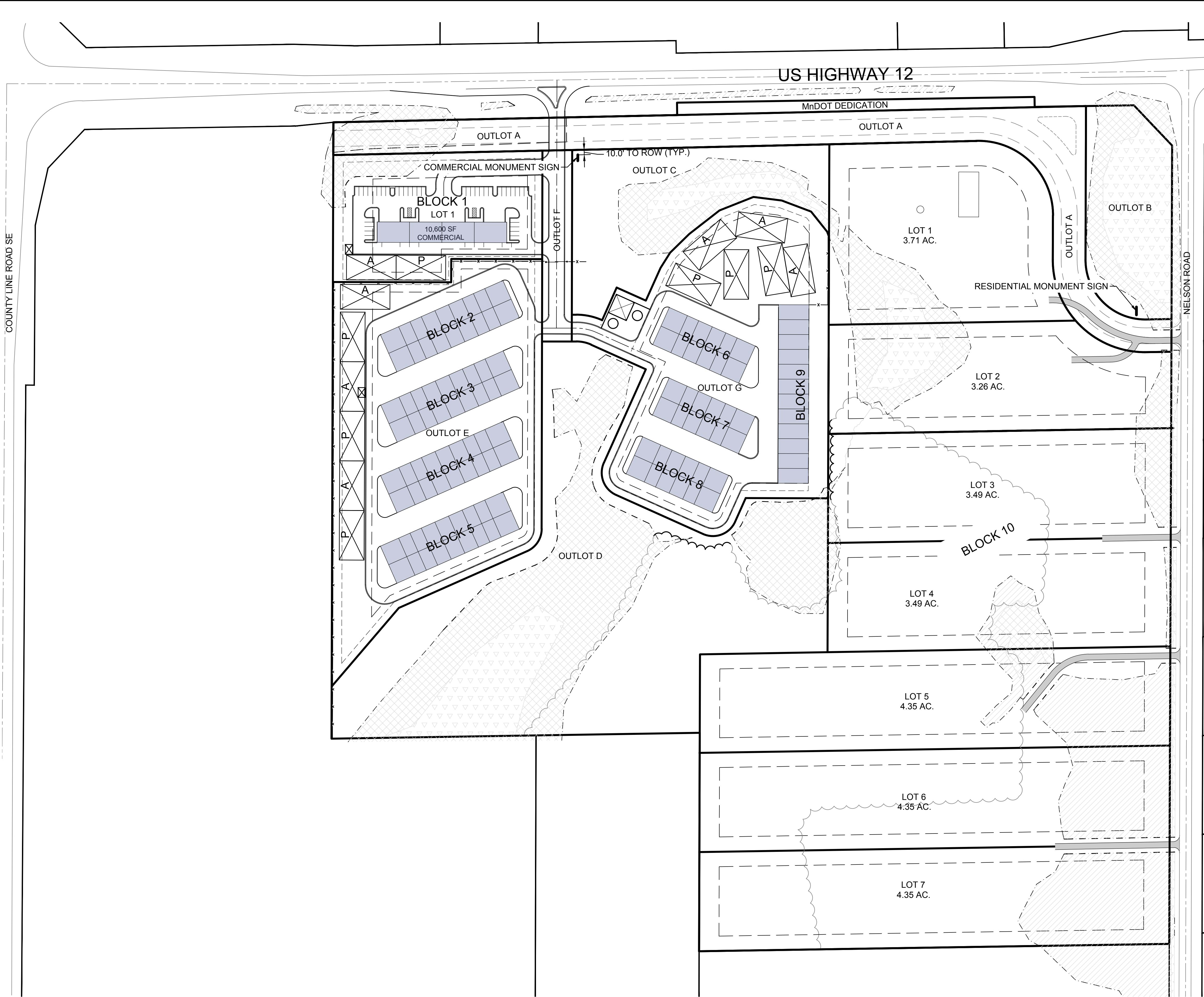
ORIGINAL ISSUE:
02/10/23
REVISIONS:
No. Description Date

221716
PROJECT NUMBER
JZ
DRAWN BY
ESG
CHECKED BY
KEY PLAN

EXTERIOR RENDERINGS -
COMMERCIAL

A3.4

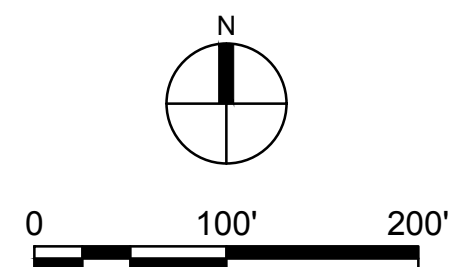
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Y:\\16900\\16915 STODDARD - 58 ACRE - INDEPENDENCE_07 Civil_01 CAD files\\01 SHEETS\\16915_C_PLAT.dwg



LEGEND

	DRAIN FIELD (PRIMARY)
	DRAIN FIELD (ALTERNATE)
	BUILDING SETBACK
	PARKING SETBACK
	EX. WETLAND BOUNDARY
	WETLAND FILL BOUNDARY
	PARCEL/LOT BOUNDARY
	ZONING BOUNDARY
	WETLAND (TYPE 1)
	WETLAND (TYPE 2)
	WETLAND (TYPE 3)

- NOTES**
1. COMMERCIAL TWO SIDED SIGN 80 SF MAX. SETBACK 10 FT FROM ROW.
 2. RESIDENTIAL TWO SIDED SIGN 32 SF MAX.
 3. SIGNAGE TO CONFORM TO CITY SIGN REGULATIONS SECTION NO. 550.



ANDERSON

13605 1st Avenue N. #100
Plymouth, MN 55441 | ae-mn.com
P 763.412.4000 | F 763.412.4090
Anderson Engineering of Minnesota, LLC

MIXED USE DEVELOPMENT

INDEPENDENCE,
MINNESOTA

STODDARD COMPANIES

REVISION LOG

NO.	DATE	DESCRIPTION OF REVISIONS

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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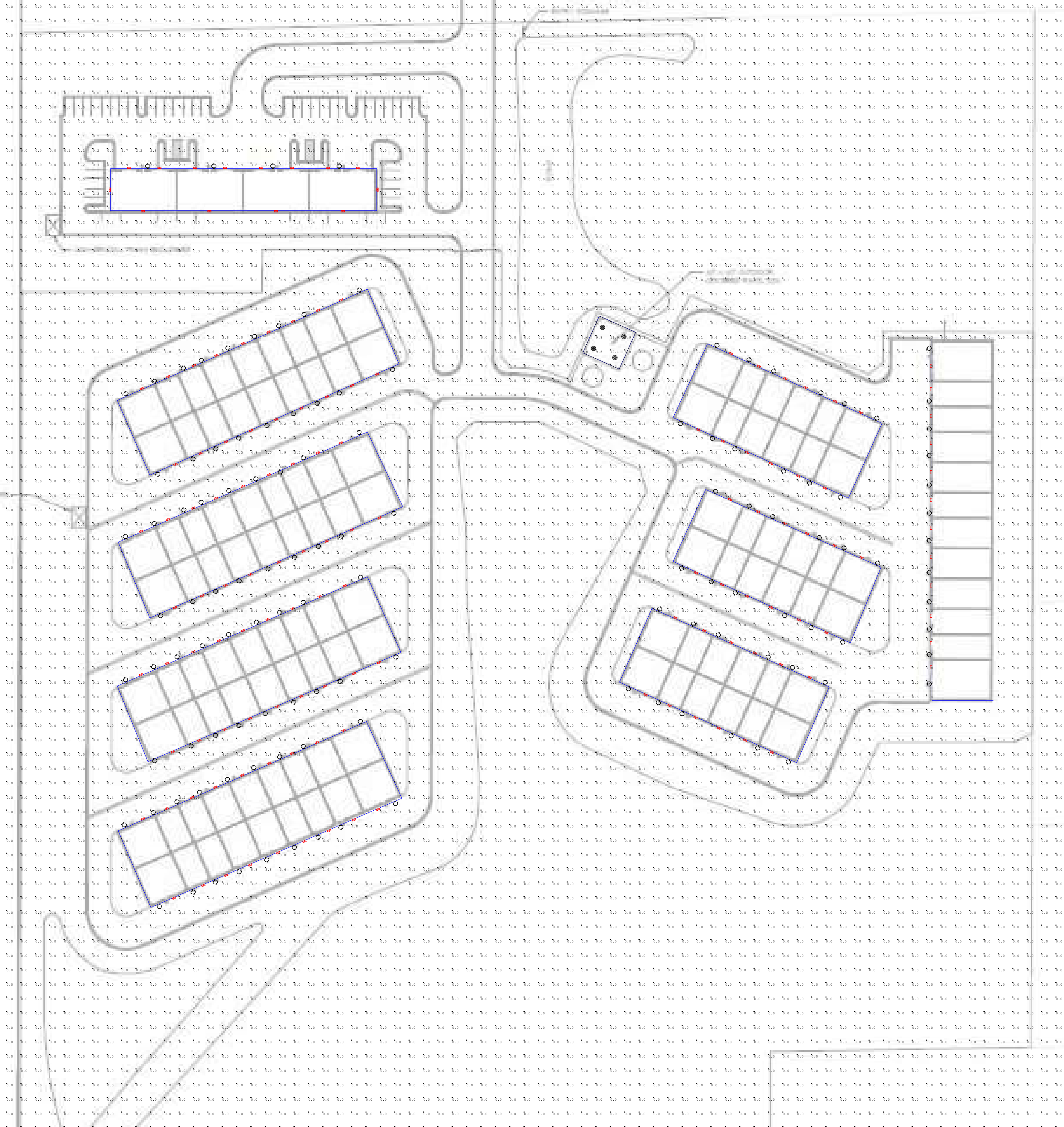
DRAWING TITLE

SIGNAGE PLAN

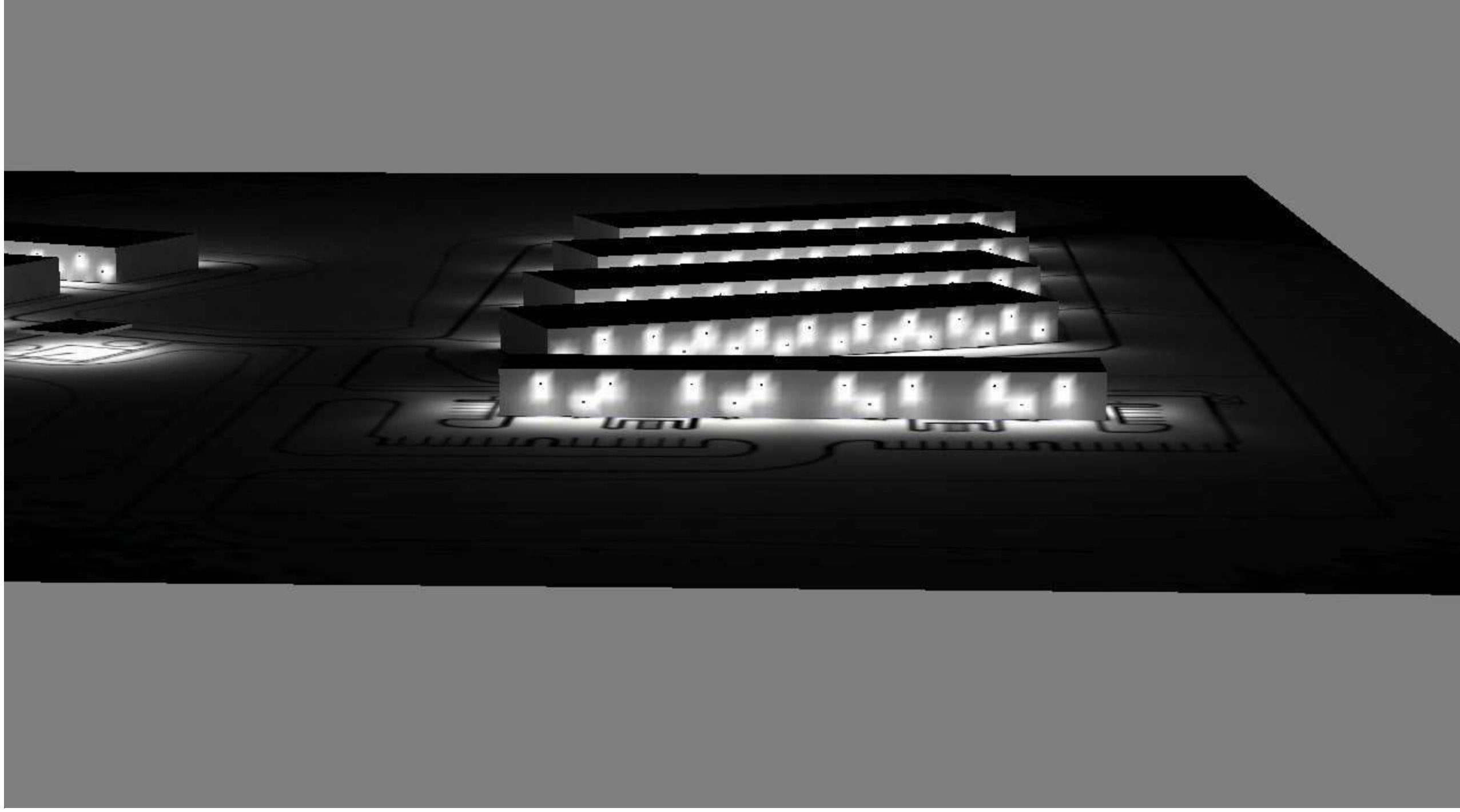
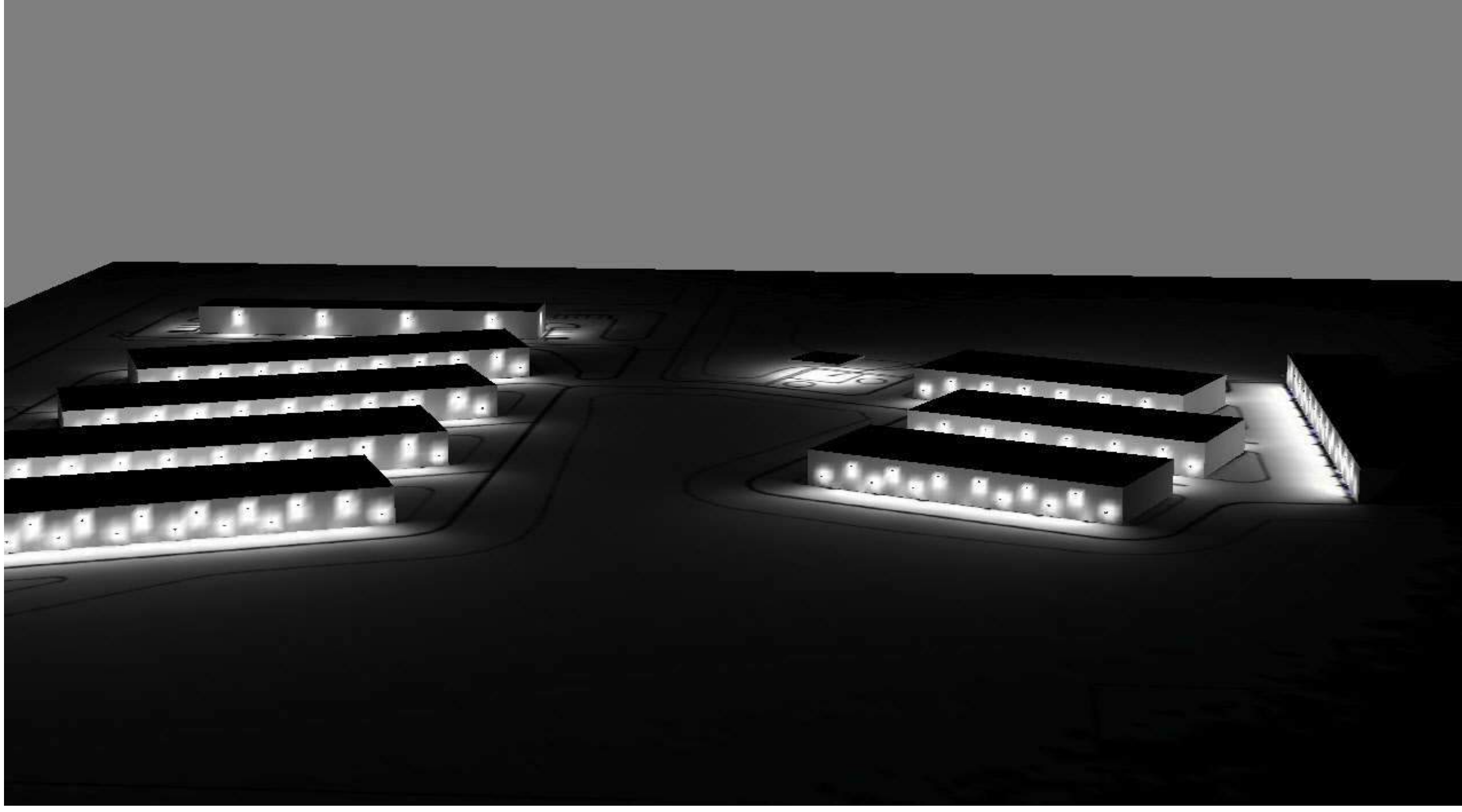
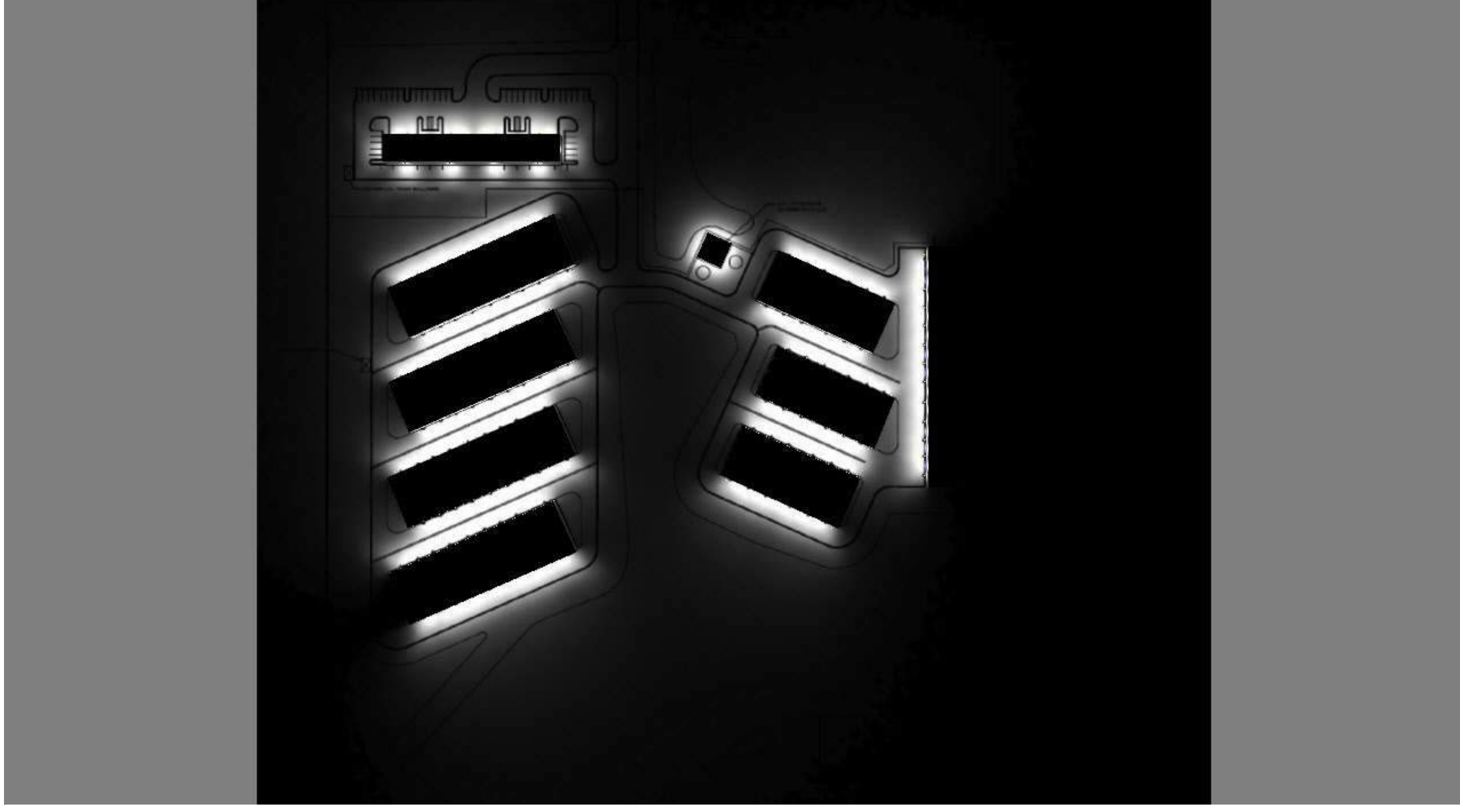
DRAWING NO.

C104

PLOTTED: 02/10/2023	COMM. NO. 16915
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Scale: 1 inch= 60 Ft.

[illegible]



Color: Bronze

Weight: 6.8 lbs

Project:

Type:

Prepared By:

Date:

Driver Info

Type	Constant Current
120V	0.40A
208V	0.30A
240V	0.20A
277V	0.15A
Input Watts	37.7/38.2/38.2W

LED Info

Watts	40W
Color Temp	3000K/4000K/5000K
Color Accuracy	80 CRI
L70 Lifespan	100,000 Hours
Lumens	4595/5421/4826 lm
Efficacy	121.9/141.8/126.5 lm/W

Technical Specifications**Field Adjustability****Field Adjustable:**

Color temperature selectable by 3000K, 4000K and 5000K

Compliance**UL Listed:**

Suitable for wet locations

IESNA LM-79 & LM-80 Testing:

RAB LED luminaires and LED components have been tested by an independent laboratory in accordance with IESNA LM-79 and LM-80

Dark Sky Conformance:

Conforms to (allows for conformance to) the IDA's fully shielding requirement, emitting no light above 90 degrees (with the exclusion of incidental light reflecting from fixture housing, mounts, and pole).

IP Rating:

Ingress protection rating of IP65 for dust and water

DLC Listed:

This product is listed by Design Lights Consortium (DLC) as an ultra-efficient premium product that qualifies for the highest tier of rebates from DLC Member Utilities. Designed to meet DLC 5.1 requirements.

DLC Product Code: PL3FKMCN05F0

Performance**Lifespan:**

100,000-Hour LED lifespan based on IES LM-80 results and TM-21 calculations

Wattage Equivalency:

Equivalent to 200W Metal Halide

LED Characteristics**LEDs:**

Long-life, high-efficiency, surface-mount LEDs

Electrical**Driver:**

Constant Current, Class 2, 120-277V, 50/60 Hz, 120V: 0.40A, 208V: 0.30A, 240V: 0.20A, 277V: 0.15A

Dimming Driver:

Driver includes dimming control wiring for 0-10V dimming systems. Requires separate 0-10V DC dimming circuit. Dims down to 10%.

Photocell:

120-277V selectable photocell that can be turned on and off.

Construction**Cold Weather Starting:**

The minimum starting temperature is -40°C (-40°F)

Maximum Ambient Temperature:

Suitable for use in up to 50°C (122°F)

Technical Specifications (continued)

Construction

Housing:

Precision die-cast aluminum housing and door frame.

Lens:

Polycarbonate lens

Mounting:

Hinged wiring access and conduit entries on the back, slides, top and bottom make installation a snap

Cut Off:

Full cutoff (0°)

Finish:

Formulated for high durability and long-lasting color

Green Technology:

Mercury and UV free. RoHS-compliant components.

Other

Note:

All values are typical (tolerance +/- 10%)

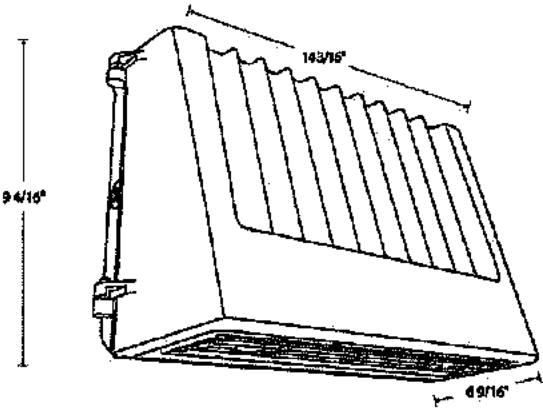
5 Yr Limited Warranty:

The RAB 5-year, limited warranty covers light output, driver performance and paint finish. RAB's warranty is subject to all terms and conditions found at rablighting.com/warranty.

Buy American Act Compliance:

RAB values USA manufacturing! Upon request, RAB may be able to manufacture this product to be compliant with the Buy American Act (BAA). Please contact customer service to request a quote for the product to be made BAA compliant.

Dimensions



Features

- Selectable CCT
- Full cutoff
- Selectable on/off photocell
- 0-10V dimming, standard

Ordering Matrix

Family	Style	Wattage
SLIM17FA	FC	40
FC = Full cutoff		40 = 40W
		60 = 60W



Economical cylinders with field adjustable options to fit any indoor or outdoor application.

Color: Black

Weight: 3.3 lbs

Project:

Type:

Prepared By:

Date:

Driver Info

Type	Constant Current
120V	0.18A
208V	0.11A
240V	0.09A
277V	0.08A
Input Watts 20W	

LED Info

Watts	20W
Color	Field Adjustable
Temp	5000K/4000K/3000K
Color Accuracy	80 CRI
L70 Lifespan	100,000 Hours
Lumens	1,693 lm
Efficacy	94

Technical Specifications

Performance

Description:

This 4-inch CD34 cylinder with integrated photocell is ideal for wall mounting in outdoor applications. Field adjustable light output (up/down/both) and color temperature (5000/4000/3000K).

Product Type:

Economy Cylinder

Wattage Equivalency:

Equivalent to 100W Incandescent

Input Wattage:

20W

Lumens (Nominal):

1,693 Lm

Efficacy:

94 Lm/W

Note:

Lumens and efficacy values are based on the highest wattage output at 3000K

L70 Lifespan:

100,000-Hour LED lifespan based on IES LM-70 results

Electrical

Driver:

Constant Current, Non-Isolated Driver, 50/60 Hz, 120-277V, 120V: 0.18A, 208V: 0.11A, 240V: 0.09A, 277V: 0.08A

THD:

<20%

Power Factor:

>0.9

Dimmable:

No

Input Voltage:

120-277V

Operating Frequency:

50/60Hz

Technical Specifications (continued)

Electrical

Operating Temperature:

-22°F - 122°F (30°C - 50°C)

Flicker:

<30%

LED Characteristics

LEDs:

LED array provides a uniform source with high efficiency and no pixilation

Color Temperature:

Field Adjustable: 5000K/4000K/3000K

Color Accuracy (CRI):

80 CRI

R9 Value:

High color performance with R9 less than or equal to 1

Color Consistency:

<5-step MacAdam Ellipse binning to achieve consistent fixture-to-fixture color

Installation

Mounting:

Wall mount

Construction

Size:

4"

Shape:

Cylinder

Finish:

Formulated for high durability and long-lasting color

Housing Material:

Cylinder aluminum extrusion/cover aluminum die casting

Gaskets:

Gasket included can achieve restrictive airflow and wet location without any additional gasket or caulk

Lens:

Regressed lens constructed from impact resistant polycarbonate. Convex lens gives the optic a similar lamp-like appearance. Provides smooth and diffuse light distribution.

Lens Finish:

Clear

Optical

Beam Angle:

50°

Compliance

Indoor/Outdoor:

Suitable for indoor and outdoor use

Environment:

Suitable for use in dry, damp, and wet environments

Fixture Rating:

Open

ENERGY STAR V2.2:

ENERGY STAR® Version 2.2 Certified

Energy Star ID:

2400418

Energy Star Model Number:

CD1004(CD34FA4W-20-508-KC)

California Energy Commission (CEC) Status:

Not lawful for sale in California

IESNA LM-79 & LM-80 Testing:

RAB LED luminaires and LED components have been tested by an independent laboratory in accordance with IESNA LM-79 and LM-80

RoHS:

Mercury and UV free. RoHS-compliant components.

FCC:

Complies with Part 15 of the FCC Rules

Other

5 Yr Limited Warranty:

The RAB 5-year, limited warranty covers light output, driver performance and paint finish. RAB's warranty is subject to all terms and conditions found at rablighting.com/warranty.

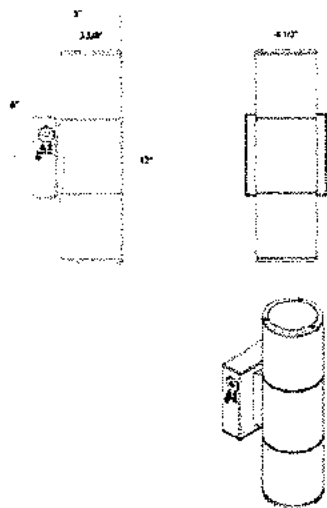
Note:

All values are typical (tolerance +/- 10%)

Buy American Act Compliance:

RAB values USA manufacturing! Upon request, RAB may be able to manufacture this product to be compliant with the Buy American Act (BAA). Please contact customer service to request a quote for the product to be made BAA compliant.

Dimensions



Features

- Aluminum extrusion housing and die-casting covers
- Field adjustable light output, select from up/down/both
- Field adjustable color temperatures between 5000/4000/3000K
- Integrated photocell
- IP65 rated

Ordering Matrix

Family	Size	Mounting	Wattage Distribution	Optic	Color Temp	CRI	Finish	Voltage	Lens
CD34FA	4	W	20	50		8	K		C
	3 = 3"	W = Wall Mount	10 = 5W up/5W down (3" only)	50 = 50° beam ¹	Blank = 5000K/4000K/3000K Selectable	8 = 80 CRI	W = White K = Black Z = Bronze	Blank = 120-277V	C = Clear
	4 = 4"		20 = 10W up/10W down (4" only)	70 = 70° beam ²					
	6 = 6"		35 = 18W up/18W down (6" only)						

¹ 3" and 4" models only available in 50Å°
² 6" models only available in 70Å°



Color: Bronze

Weight: 7.0 lbs

Project:

Type:

Prepared By:

Date:

Driver Info

Type	Constant Current
120V	0.39A
208V	0.22A
240V	0.19A
277V	0.17A
Input Watts	46W

LED Info

Watts	45W
Color Temp	5000K (Cool)
Color Accuracy	73 CRI
L70 Lifespan	100,000 Hours
Lumens	6,045 lm
Efficacy	131.4 lm/W

Technical Specifications

Compliance

UL Listed:

Suitable for wet locations

IP Rating:

Ingress protection rating of IP66 for dust and water

Title 24 Compliant:

An IVEPARK edgelit garage fixture with a WS2 control option can be used to comply with 2016 Title 24 Part 6 Section 130.2 (a,b,c)

IESNA LM-79 & LM-80 Testing:

RAB LED luminaires and LED components have been tested by an independent laboratory in accordance with IESNA LM-79 and LM-80

DLC Listed:

This product is listed by Design Lights Consortium (DLC) as an ultra-efficient premium product that qualifies for the highest tier of rebates from DLC Member Utilities.
DLC Product Code: PL28GIXV

LED Characteristics

LEDs:

Long-life, high-efficacy, surface-mount LEDs

Color Stability:

LED color temperature is warrantied to shift no more than 200K in color temperature over a 5-year period

Color Uniformity:

RAB's range of Correlated Color Temperature follows the guidelines of the American National Standard for Specifications for the Chromaticity of Solid State Lighting (SSL) Products, ANSI C78.377-2017.

Performance

Lifespan:

Up to 100,000-Hour LED lifespan based on IES LM-80 results and TM-21 calculations

Wattage Equivalency:

Equivalent to 150W Metal Halide

Construction

Cold Weather Starting:

The minimum starting temperature is -20°C (-4°F)

Maximum Ambient Temperature:

Suitable for use in up to 40°C (104°F)

Mounting:

Surface mount standard (accessories available for wall, pole and trunnion mount)

Lens:

Diffused Polymethyl Methacrylate (PMMA)

Technical Specifications (continued)**Construction****Finish:**

Formulated for high durability and long-lasting color.

Green Technology:

Mercury and UV free. RoHS-compliant components.

Other**Warranty:**

RAB warrants that our LED products will be free from defects in materials and workmanship for a period of five (5) years from the date of delivery to the end user, including coverage of light output, color stability, driver performance and fixture finish. RAB's warranty is subject to all terms and conditions found at rablighting.com/warranty.

Buy American Act Compliance:

RAB values USA manufacturing! Upon request, RAB may be able to manufacture this product to be compliant with the Buy American Act (BAA). Please contact customer service to request a quote for the product to be made BAA compliant.

Electrical**Driver:**

Class 2, 50/60Hz, 120-277V, 4kV standard, 10kV optional, 120V: 0.38A, 208V: 0.22A, 240V: 0.19A, 277V: 0.17A

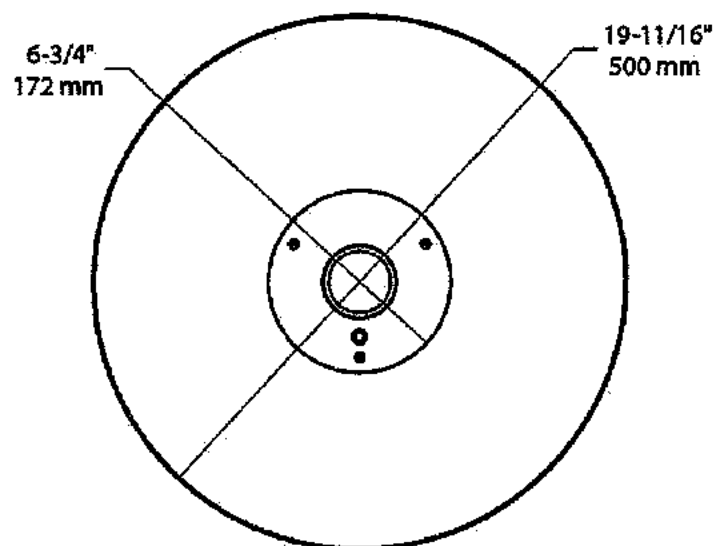
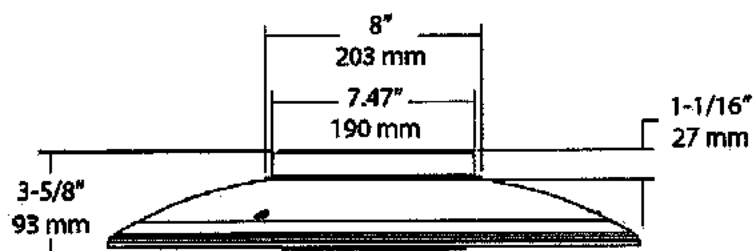
THD:

13.18% at 120V, 13.27% at 277V

Power Factor:

98.8% at 120V, 95.33% at 277V

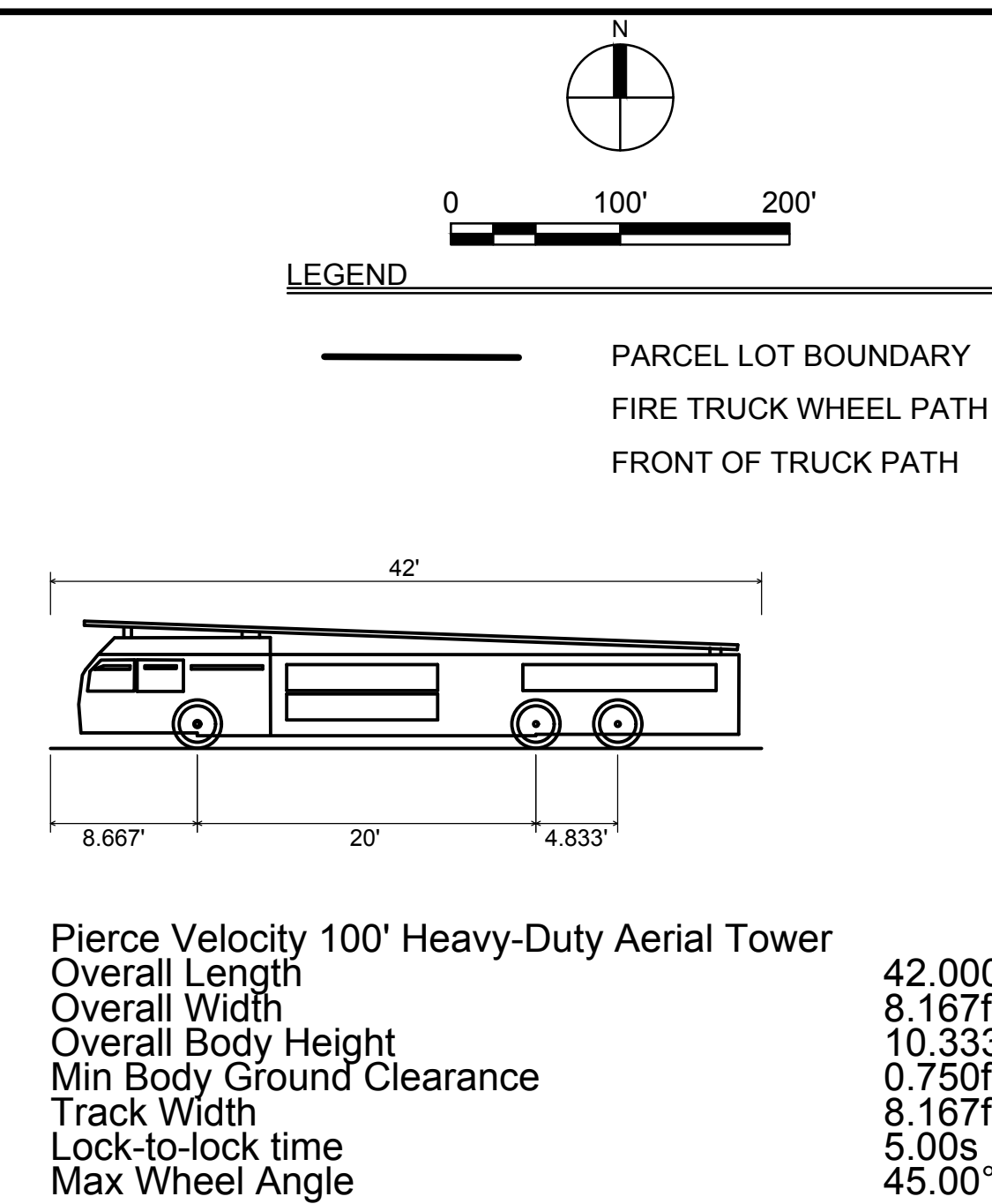
Dimensions



Ordering Matrix

Family	Distribution		Lumen Output	CRI/Color Temp	Finish	Voltage/Driver	Sensor Options	Lightcloud	Emergency Option
IVG	T5	-	50L	750	Z	U			
	T5 = Type V Wide		30L = 3,600 lumens (28W)	750 = 70CRI 5000K	Z = Bronze	U = 120-277V 0-10V Dimming	Blank = No Options /WS = 8ft lens Wattstopper	Blank = No Lightcloud*	Blank = No Options
	T5C = Type V Concentrated		50L = 5,500 lumens (45W)	740 = 70CRI 4000K	W = White	4 = 480V 0-10V Dimming ³	/WS2 = 20ft lens Wattstopper	/LC = Lightcloud* Controller ¹	/E2 = Emergency ²
	T5U = Type V Wide with Uplight		70L = 7,500 lumens (69W)	730 = 70CRI 3000K					
	T5CU = Type V Concentrated with Uplight								

¹ Available only for T5 and T5C distribution² Applies to 120-277V³ Consult Factory



MIXED USE DEVELOPMENT

INDEPENDENCE,
MINNESOTA

STODDARD COMPANIES

REVISION LOG

[illegible]

PRELIMINARY

DESIGNED: LRK	DRAWN: BN/MM	CHECKED BY: LRK
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DRAWING TITLE

FIRE TRUCK TURNING EXHIBIT

DRAWING NO.

A

PLOTTED: 02/10/2023	COMM. NO. 16915
------------------------	--------------------



January 30, 2023

Bill Stoddard
Stoddard Companies
925 Excelsior Blvd.
Excelsior, MN 55331

RE: Site Plan Review and Associated Zoning Submittals – 9285 Highway 12

Dear Bill:

The City received an initial submittal for the development of the subject property on November 16th, 2022, and a second updated submittal on December 5th, 2022. The City subsequently received several additional items on December 16th, 2022 but has not yet received all documents requested in our last correspondence. The City has completed a detailed review of the information submitted and pertaining to the following applications:

- A Comprehensive Plan Amendment to allow a portion of the property to be re-guided to the Urban Commercial - UC zoning district. A portion of the property is already guided Urban Commercial.
- Rezoning of a portion of the property from AG-Agriculture to UC-Urban Commercial.
- Conditional Use Permit to allow a planned unit commercial development on the subject property. The proposed development would include office warehouse and private garage condominium buildings.
- Site Plan Review to allow the proposed development.
- Preliminary Plat to allow the subdivision of the property into eight (8) residential lots and associated lots to allow the proposed commercial development.

As a formality and in accordance with Minnesota State Statute 15.99, the City is required to notify you that the applications are still found to be incomplete due to not all items identified in the previous correspondence being submitted. There are still several items/plans that are required to be submitted in order for the applications to be considered complete. Those items are further detailed below.

- Geotechnical Report
- Lighting Plan
- Dimensioned Site Plan

I offer the following comments for your review and possible revision:

Applicant has provided responses in red text to each comment (see below):

1. A dimensioned site plan was not submitted with the initial or subsequent plan submittals. Please provide a fully dimensioned site plan that provides dimensions of the proposed roads and driveways, parking spaces, turning radius, building dimensions, etc.

A dimensioned Site Plan is attached in Exhibit No. 1, Anderson Engineering's Civil Engineering Plan Set.

2. The overall plan provides proposed setbacks for parking, residential and commercial building setbacks. Please note that the residential lots will be zoned AG-Agriculture and subject to all applicable building setbacks and lot standards pertaining to the AG zoning district. For the commercial development, the City would recommend the following building and parking setbacks:

Applicant has updated Building and Parking Setbacks to generally agree with recommendations. However, there are some alternate setbacks dimensions we are proposing to minimize wetland impacts.

	<u>Recommended</u>	<u>Proposed</u>
<u>Building Setbacks</u>		
Front yard setback:	50 ft (including corner yard setbacks)	50 ft
Side yard setback:	20 feet	20 ft
Rear yard setback:	40 feet Setback from boundary of agricultural or residential district: 100 feet	80 ft from agricultural 40 ft from residential
<u>Parking and Drive Aisle Setbacks</u>		
Front yard setback:	20 feet	15 ft (from future frontage road)
Side yard setback:	10 feet	5ft - 15 ft (varies)
Rear yard setback:	10 feet	5ft - 15 ft (varies)
Setback from boundary of agricultural or residential district:	20 feet	20 ft from agricultural 40 ft from residential

3. The proposed principal access drive off of Highway 12 and the future frontage road stubs are not dimensioned and it is unclear if these are intended to be dedicated to the public at some point now or in the future? It may be necessary for right of way to be dedicated which could accommodate a future public frontage road. The minimum width of a City right of way is 66'.

There is a narrow portion of land along the south side of Highway 12 that is anticipated to be dedicated to MnDOT with the plat (See green hatch on Dedication Plan). We have also reserved a 66-ft wide strip of land along the south side of Highway 12 (See blue hatch on "Dedication Plan", also identified as Outlot A on Preliminary Plat) to be dedicated to the City for the future construction of a public frontage road from Nelson Road to the west property line of this proposed development, and potential extension across the adjacent property to the west to provide a connection to County Line Road SE.

4. The City will want to ensure fire apparatus access to and around the proposed retail and lifestyle condominiums. The City is in the process of reviewing the plans with the fire department and will provide additional comments once more detail is provided. The City will want to verify that the specified fire apparatus can maneuver into, through and around the proposed site and buildings.

We contacted Delano Fire Chief, Bob VanLith, who informed us that their largest fire truck is a Pierce, Velocity model with 100' midmount platform ladder. We used AutoTurn software to ensure our design of the private roads accommodate this design vehicle.

5. The City requires all paved areas serving commercial development to be paved and curbed with B-6-12 or better concrete curbing. The plans are not clear on what is proposed and where. The City could consider elimination of the curb provisions inside the lifestyle auto condos area.

Curbing is now detailed on our Anderson Engineering Plan package as attached. The design includes B618 curb & gutter for the right-in/right-out island within the MnDOT right-of-way in accordance with MnDOT specifications, B612 curb & gutter within the commercial area and main entry drive, and surmountable curb around the outside perimeter of all private drives within the Auto Condo area.

6. The north south lifestyle auto condo building located on Lot 11 appears to be located approximately 25' from the east property line which separates the commercial and residential property. The City would like to maintain minimum setbacks between commercial and residential as noted above. There appear to be alternative ways to accomplish an increased setback.

We have increased subject Block 9 Auto Condo building east property setback to 40 ft and are also adding a Restrictive Covenant to the newly created residential lots to prohibit the cutting of healthy trees within 150 ft of their rear property line/subject property line. Refer to attached Exhibit No. 2, Proposed Residential Lot Restrictive Covenants.

7. The City's current commercial ordinance requires a landscape island for every commercial parking lot that has more than 20 consecutive parking spaces. The City would like to see landscape islands installed along the north side of the proposed commercial building. All landscape islands shown should have a minimum interior width of 8' to accommodate a deciduous tree.

Landscape Islands have been added of appropriate size with vegetation per attached Plans.

8. The plans indicate a dumpster enclosure in the southwest corner of the proposed commercial lot. More information should be provided relating to the proposed enclosure, including materials, size, etc.

Two total dumpsters (one on commercial site and one on auto condo site) have been added to our development per the attached plans. Dumpsters shall have lids and full enclosures on all four sides with a latched gate on the front of both dumpsters built with siding materials complementing their respective main building components.

9. The lifestyle auto condos do not indicate any centralized waste or trash collection point. Please provide additional information relating to trash collection for the proposed lifestyle auto condo development.

An auto condo central dumpster enclosure has been added (Refer to above Answer 8).

10. More detail will be required to be added to the proposed landscape plan. Generally, the City will want to establish a high level of landscaping along Highway 12 and within the proposed commercial development property. This would include street or access driveway deciduous overstory trees, entry landscaping, landscaping against the building, groundcover, landscape islands, etc. Please label all plants and provide a table indicating species, size, condition, etc.

We have revised the Landscape Plan and added a table that meets City desires attached.

11. The plans show an entrance gate into the proposed lifestyle auto condos. The City noted that a fence or security may be necessary between the lifestyle auto condos and the adjacent residential property and also along the north side of the property to separate and secure the condo area. The inclusion of a security fence should be further considered and discussed.

We have added a security fence along the north, east and west sides as depicted in attached plans. Auto Condo owners and City safety and fire personal will have codes to security gate.

12. Please provide information relating to the proposed commercial building architecture, floor plan, entries, overhead doors, etc. In addition, mechanical equipment will need to be screened and should be shown on the proposed plans. Please provide more detail relating to the proposed commercial building. The City will want to review proposed building architecture and generally configuration of the interior spaces.

Updated Commercial Building design, renderings and elevations by ESG Architecture & Design have been submitted as Exhibit No. 3 for your review. Mechanical equipment will be either rooftop or ground mounted for the commercial building. For the auto condo building heated floors with individual boilers will be located inside each respective unit, with air conditioning, if any, likely either be wall mounted mini-split systems or roof mounted. All mechanical equipment shall be screened from neighboring views.

13. A lighting plan was not provided. A lighting plan (photometric) will need to be submitted. The plan should indicate the proposed light fixtures, pole heights, and photometric readings. Cut sheets of all proposed fixtures should also be submitted.

535.07. General application.

- Subd. 1. All light sources must be installed and shielded to prevent glare and trespass light. All light sources must be installed and shielded in accordance with the requirements of subsection 535.11 of this Code.*

- Subd. 2. Glare is prohibited in all districts.*
- Subd. 3. Trespass light must not exceed five-tenths footcandle, as measured on the property line when abutting any residential lot, and one footcandle on any abutting commercial or industrial lot, between the hours of sunset and sunrise.*
- Subd. 4. No light source may be mounted or installed at a height above the ground exceeding 35 feet.*
- Subd. 5. Blinking, flashing, or changing intensity lights are not permitted, except as authorized by subsection 550.07, subdivision 1 of this Code.*
- Subd. 6. It is the responsibility of every installer of lighting and every owner or occupant of property on which a light source is located to comply with the provisions of these lighting regulations.*

A lighting & Signage Plan by ESG Architecture & Design has been added and attached as Exhibit No. 4. Cut Sheets of proposed fixtures (or equivalent) have also been submitted along with a Photometric Plan. Lighting shall be installed and shielded in accordance with the requirements of City Code 535.11. No parking lot “pole mounted” lights are proposed.

14. The preliminary plat should indicate the edge of delineated wetlands, wetland buffers, applicable drainage and utility easements and allowable building pads (for all commercial and residential lots).

Preliminary Plat has been updated to include edge of delineated wetlands. We have not yet added wetland buffers to plans, but intend to do so with our next submittal. The proposed plat will locate all existing wetlands and buffers to remain and proposed stormwater ponds within Outlots C and D with a blanket drainage and utility easement across each of the outlots. All commercial building pads are shown and dimensioned on the Preliminary Plat drawings. We are not proposing any defined building pads for the residential lots, as we would like to maintain flexibility required to locate septic fields after soils investigations are performed in the field.

15. The proposed residential lots should show the location on the plan where a primary and secondary septic site can be located along with a corresponding report verifying the locations.

Site locations will be determined by field investigation with report submitted to City in Spring when field conditions permit entry.

16. The City’s current residential ordinance requires 250 LF of frontage for all lots greater than 3.49 acres. This would include Lots 1, 2 & 6-8. Lots 3-5 comply with the frontage requirements. The applicant could revise the plan to reduce the acreage of the proposed lots or increase the frontages to meet the specified requirements. The applicant is attempting to mirror the lots across the street; however, those lots are all less than 3.49 acres in size.

We have reduced the proposed residential lot configuration to seven (7) lots fronting Nelson Road to bring lots 2-4 into conformance with City Ordinance. However, we are requesting flexibility under the PUD application for Lots 5-7 to mirror the lot widths to the east of Nelson Road as we feel this is a better Plan than adding another Outlot to the rear of those subject Lots or making the auto condo site larger. Future residents will LOVE the larger lot sizes anyway! And as earlier mentioned we are adding a Restrictive Covenant to prevent tree cutting from the rear 150 ft of all residential lots.

17. Please find attached, the engineering review comments prepared by Hakanson Anderson and dated, January 27, 2023.

We have addressed most of the January 27, 2023, Hakanson Anderson comments with our revised Civil Plans by Anderson Engineering. As you are aware, some forthcoming comments by us will require geotechnical findings and detailed boundary survey dimensions. However, current winter conditions will delay some of these efforts until Spring. We have used County GIS parcel mapping and LiDAR topographic data as a basis for our initial design. We intend to initiate a geotechnical study shortly along with an ALTA survey with complete boundary and topographic information this Spring as soon as weather permits.

18. Please find attached, MNDOT review comments dated, January 12, 2023.

We are in basic agreement with the updated MnDOT comments regarding Hwy 12 site access and elimination of the planned acceleration lane per their February 3, 2023 correspondence. If any Pedestrian/Bicycle lane is to be added along this south Hwy 12 site we insist it be located within the newly dedicated 66 Ft wide ROW land and adjacent land we are dedicating to MnDOT. We will conform with MnDOT's water resource rules and other review and permit procedures and requirements.

Please review this letter and make changes and revisions necessary to address all applicable comments. Please provide a point-by-point response letter along with revised plans and subsequent information as requested no later than Friday, February 10, 2022. Following the review of additional information or revisions as requested, the City may have additional comments or require additional information. It is noted that the City and applicant have had discussions with MNDOT relating to the site access. Further review of the plans by the City may be dependence on the level of resolution relating to the site access issue. With this noted, should the City and applicant identify an acceptable access resolution, it is anticipated that this application will be considered again at the February 21, 2023, Planning Commission Meeting. If you have any questions, please contact me at (763) 479-0044.

We greatly appreciate your time in consideration and review of our attached information as we strive to be the premier business park & Upscale Life-Style Auto Condo and Residential development in the greater area and will forward any additional information as requested by city in prompt fashion.

Sincerely,

William J. Stoddard & Team

Sincerely,



Mark Kaltsas, PLA
City Planner



**ENGINEERING REVIEW
by
Hakanson Anderson**

Submitted to: Mark Kaltsas, City Administrator

**cc: SH Ventures, Inc., Applicant
Lee Koppy, Anderson Engineering of MN**

Reviewed by: Shane Nelson, P.E.

Date: January 27, 2023

Proposed

Project: Mixed Use Highway 12 Development

Street Location: 2858 Nelson Road

Applicant: SH Ventures, Inc.

Owners of Record: John F Zeglin JR

**Jurisdictional Agencies: City of Independence, MnDOT, BWSR, MnDNR, MPCA,
(but not limited to) PSCWMC**

**Permits Required: City Approval, MnDOT Access Permit, NPDES
(but not limited to) Construction Permit, WCA Wetland Replacement Plan,
PSCWMC Permit, Water Appropriations Permit**

INFORMATION AVAILABLE

Development Narrative

Preliminary Plans, dated November 15, 2022, prepared by Anderson Engineering of Minnesota, LLC. – Updated Preliminary Plans are dated February 10, 2023.

Stormwater Management Plan, dated December 16, 2022, prepared by Anderson Engineering of Minnesota, LLC. – Updated Stormwater Management Plan will be prepared as soon as geotechnical study is complete and better soils information is available including infiltration rates and groundwater elevations.

Wetland Delineation Report, dated May 6, 2022, prepared by Anderson Engineering of Minnesota, LLC.

Trip Generation Study, dated November 14, 2022, prepared by S² Traffic Solutions.

SITE ACCESS / VEHICULAR TRAFFIC

1. The commercial and auto condos portion of the project is proposed to receive access from Highway 12, which is under MnDOT jurisdiction. The Applicant shall be required to obtain a permit from MnDOT for the improvements necessary to support this development. The new street access must meet all design and safety criteria as per MnDOT requirements and the Applicant shall be responsible for providing a design that meets MnDOT standards and shall be solely responsible for all costs associated with the construction of the new access and associated turn lanes, including right-of-way acquisition if necessary.

We understand the requirement and fully intend to obtain a permit from MnDOT for the improvements. We further understand the requirements stated above related to safety criteria and responsibility for payment for such improvements.

2. The preliminary plans depict a frontage road connection to the (Johnson Trustee) property to the west, which is supported by Staff. It is assumed that the frontage road will be Privately Owned and a cross easement will be recorded to provide legal access to the adjoining property, which could then be reciprocated at the time the adjoining property is developed.

We are proposing to dedicate 3.23 Acres of land along the north property line to the City of Independence for the future construction of the proposed frontage road. We believe the frontage road should be a public roadway.

3. We would recommend that the access aisles around the car condos are designed to accommodate emergency service vehicles. A turning radius exhibit for a fire truck maneuvering the site should be provided for City review. We recommend that a fire truck is able to navigate around the site with single-turn maneuvers without the need to reverse.

We contacted Delano Fire Chief, Bob VanLith, who informed us that their largest fire truck is a Pierce, Velocity model with 100' midmount platform ladder. We used AutoTurn software to ensure our design of the private roads accommodate this design vehicle.

4. The proposed single family lots are proposed to access Nelson Road via shared driveways (to reduce wetland impacts). Staff generally supports this.

We have maintained the design of shared driveways, and further refined to reduce the total number of access points to four (4), reduced from the previous total of six (6) driveways on the previously most recent version Preliminary Plat submitted on 12/16/2022.

PRELIMINARY PLAT

1. The Preliminary Plat is not certified by the preparer. A certified Preliminary Plat is required to be submitted.

Anderson Engineering has used County GIS parcel mapping and LiDAR topographic data as a basis for our initial design. We would prefer to refrain from signing the Preliminary Plat document until we have completed a boundary and topographic survey of the property.

2. The Preliminary Plat shall depict zoning classifications.

We have added the existing zoning and guiding designations to the Preliminary Plat.

3. The wetlands shall be labeled as Exceptional, High, Medium, or Low and appropriate buffer widths depicted on the Preliminary Plat.

The Wetland Mitigation Plan, Sheet C500 includes the designations of Type I, Type II, Type III for the existing wetlands. We will add the buffer widths to the plans with the next issue of the plans.

4. Drainage and utility easements shall depicted on the Preliminary Plat. Drainage and utility easements shall be provided to the HWL of the wetlands, or to the edge of the buffer, whichever is greater, and over all stormwater conveyances and stormwater ponds.

The proposed plat will locate all existing wetlands and buffers to remain and proposed stormwater ponds within Outlots C and D with a blanket drainage and utility easement across each of the outlots. All commercial building pads are shown and dimensioned on the Preliminary Plat drawings. We are not proposing any defined building pads for the residential lots, as we would like to maintain flexibility required to locate septic fields after soils investigations are performed in the field.

5. The bearings and lengths of the property boundary shall be depicted on the Preliminary Plat. The found iron monuments shall be depicted on the Preliminary Plat.

We have used County GIS parcel mapping data as a basis for our initial design. We intend to initiate a complete boundary survey by a registered land surveyor this Spring as soon as weather permits. We will add bearings and lengths and found iron monuments to the Preliminary Plat as soon as we have obtained this information.

6. The lengths of the proposed lot lines shall be labeled on the Preliminary Plat.

See response to #5 immediately above.

GRADING AND EROSION CONTROL

1. As per the wetland and stormwater comments below, revisions to the Grading Plan are likely necessary to comply with City Code requirements, Watershed Rules, and the NPDES Permit. Please revise the Grading Plan as required to conform to all applicable requirements.

We will continue to refine the Grading and Erosion

2. Proposed grading is not depicted for the residential lots. Please depict proposed grading, particularly for the shared driveways. Please also label minimum floor elevations and allowable lowest opening elevations.

We have added proposed grading required to construct shared driveways, including storm sewer culverts to maintain drainage within the existing ditch along the west side of Nelson Road. We will label minimum floor elevations and allowable lowest opening elevations as soon as we are able to complete the geotechnical study, which will in turn, allow us to complete the Stormwater Management Report for the project by determining the 100-year flood elevations for all existing wetlands.

3. Please identify locations of primary and alternate septic sites for the residential lots.

Site locations will be determined by field investigation with report submitted to City in Spring when field conditions permit entry.

4. Redundant silt fence is required for sediment control adjacent to wetlands. Please depict on the erosion control plan.

Erosion Control Plans have been updated to show redundant silt fence at the required locations.

5. A stabilized construction exit will be required to prevent sediment tracking onto Highway 12. Please depict on the erosion control plan.

Erosion Control Plans have been updated to show a rock construction entrance at the specified location.

6. A complete erosion control plan and SWPPP will be required with the final development plans.

Noted. Anderson Engineering will prepare a SWPPP for the proposed construction when the final development plans are submitted.

7. Please label normal water level and 100-year high water level on all existing wetlands and proposed basins on the Grading Plans. Final Grading Plans shall also label emergency over flow elevations and locations throughout the development.

We will add this information to the plans as soon as the geotechnical study and subsequent Stormwater Management Plan are completed.

8. Please depict access routes for maintenance of stormwater ponds and stormwater structures on the plans. Access routes shall be a minimum of 10 feet wide, a 10% maximum grade, and 10:1 maximum cross slope.

We will add the proposed maintenance access routes to the plans once we have determined that the pond sizes are sufficient. This will be determined by the completion of the Stormwater Management Plan.

9. Please depict the pre-treatment for the infiltration/filtration basins on the plans.

We will incorporate pre-treatment to the plans once we have the results of the geotechnical study to determine which pre-treatment method(s) are appropriate for this site.

WETLANDS

1. A wetland delineation report was reviewed and approved by the TEP. A Notice of Decision approving the wetland boundary and type was issued on 8/5/2022. The decision is valid for 5 years.

Noted.

2. There are several proposed impacts to existing wetlands. A Wetland Replacement Plan has not been submitted at this time. Wetland impacts are subject to a sequencing analysis in which first wetland impacts must be avoided, and if they cannot be avoided they must be minimized. Although the shared driveways for the residential lots achieves some level of minimization of those specific wetlands, minimization of the impacts associated with the commercial development / car condos has not been demonstrated. If the wetland impacts are not approved, revisions to the site layout may be necessary.

We are aware of the requirements for a Wetland Replacement Plan and the minimization requirements that go with it. The application that was submitted on November 15, 2022 was not the first design developed for this site which initially impacted over 4 acres of wetlands with the proposed sizes of our office/warehouse which we drastically reduced with Staff input. The current plans were also able to reduce the total proposed wetland fill area by more than 12-percent from 58,592 SF in the 11/15/22 submittal to 51,404 SF with this resubmittal dated 2/10/23. We intend to prepare and submit a Wetland Replacement Plan will be the result of the iterative process to minimize impacts as required by wetland protection laws as we continue to refine the design of the site.

3. Please label the wetland quality for all wetlands on the grading sheets. Please also depict quality appropriate wetland buffers and setbacks from these buffers on the Grading Plans. (Note both the City of Independence and PSCWMC have buffer requirements – buffer widths shall meet both requirements)

The Wetland Mitigation Plan, Sheet C500 includes the designations of Type I, Type II, Type III for the existing wetlands. We will add the buffer widths as dictated by both City of Independence and Pioneer Sarah Creek Watershed Management Commission to the plans with the next issue of the plans.

4. Buffer establishment plans in accordance with Section 509.05 of the City Code and PSCWMC Rules will be necessary. At this time, a Buffer Establishment Plan has not been received.

See response to #3 immediately above.

5. Increased wetland buffers will be required for steeper slopes. Buffer strips on shall be an average 25 feet wide (PSCWMC requirement). Buffer areas disturbed by grading operations must be finish graded to a slope of 6:1 or less or an increase in width of five (5) feet for each one (1) foot decrease in horizontal width (i.e. a 25 required foot buffer width at a 5:1 slope must be 30 feet wide, 4:1 must be 35 feet wide, and 3:1 must be 40 feet wide.)

Noted. We will provide buffers as required.

6. An analysis of wetland bounce between existing conditions and proposed conditions has not been completed. Please ensure that the maximum bounce and inundation period is not exceeded for the onsite wetlands for their given quality.

We will include this analysis in the Stormwater Management Plan when we resubmit the document.

STORMWATER

1. This project proposes to create eight new residential lots along Nelson Road. The Stormwater Management Plan states “we propose to defer the stormwater requirements onto individual home builders” - which is not allowable. In accordance with NPDES State permitting requirements, the eight new residential lots proposed to be created with this development are a “Common Plan of Development” and therefore design and construction of stormwater infrastructure is required at this time. The Applicant shall revise to include the design of stormwater management for the eight new lots in accordance with City ordinances and state permitting requirements.

We understand this requirement and will address with the Stormwater Management Plan when we resubmit.

2. The current stormwater report has determined that the ponds as submitted are undersized at this time and shows a significant increase in runoff rates. The Stormwater Management Plan, as submitted, does not meet City requirements for rate control.

We understand the rate control requirements and will address with the Stormwater Management Plan when we resubmit.

3. The current stormwater report states “Since we do not have detailed soils information at this time, and we are still working through pond sizing, we are unable to determine whether or not the design meets Phosphorous and TSS removal requirements”. The Applicant must demonstrate that the stormwater system is designed to reduce Phosphorous and TSS loading.

We understand the water quality requirements and will address with the Stormwater Management Plan when we resubmit.

4. Without soils information, it is unknown if the pond locations will be suitable to provide for stormwater treatment which is able to achieve the water quality requirements. There may be other areas of the site which are more suitable (undetermined at this time) which could result in a revised site layout.

Once we have completed the geotechnical study, we will be able to review soil conditions across the site to determine the optimal locations for the proposed stormwater treatment. We understand that some revisions may be required to accommodate.

5. In the existing condition, there are several existing wetlands which should be modeled with a Curve Number of 98 to reflect the saturated condition. However, it does not appear that the wetland ground cover has been accounted for in the existing condition model in any of the subcatchments. Please revise.

We will revise the report to account for the saturated condition when we resubmit.

6. In the existing condition, there is depressional storage within the wetlands that may impound stormwater during rain events within Subcatchment A1, however, the storage is not modeled in the existing conditions model. The model shall be corrected to model the depressional storage within the existing wetlands.

We will revise the report to account for these depressional storage areas when we resubmit.

7. The wetland within Subcatchment A1 is partially located on the adjacent property to the south. At this time, we are unable to evaluate whether or not there are offsite impacts due to the wetland not being included in the model. Based on the significantly increased stormwater runoff rates, we would assume that there would be an increase in the wetland high water level elevation, which is not allowable unless the Applicant receives permission from the landowner in a form suitable to the City Attorney.

It is our intent to design proposed stormwater management facilities that will not increase offsite flows or increase high water levels.

8. In the existing condition, there are existing wetlands within subcatchment B1 that are not included in the model.

We will revise the report to account for the missing existing wetlands when we resubmit.

9. In both the existing and proposed conditions, the subcatchments shall include labels to describe the ground cover associated with the Curve Number and area. Curve Numbers shall be in accordance with the City of Independence

We will include labels describing ground cover associated with the selected Curve Numbers and do so in accordance with the City of Independence guidelines.

10. The proposed Drainage Area Map does not include any ponding nodes. The Proposed Drainage Area Map shall be revised to depict ponding nodes.

Noted. We will include the proposed ponding nodes on the revised Drainage Area Map included with the resubmitted Stormwater Management Plan.

11. All wetlands, wetland buffers, stormwater structures, conveyance systems, emergency over flows, cross lot drainage, and stormwater detentions basins must be within a drainage and utility easement that contains the entire 100-year high water level. For conveyance systems, the width shall at a minimum contain the flow path and shall increase for larger conveyances. Please show these easements on the construction plans.

We will be sure to include drainage and utility easements over all wetlands, buffers, stormwater structures and other features noted.

12. Groundwater elevations are required to be determined for the suitability of stormwater BMPs. Infiltration basins require at least 3 feet of separation between the bottom of the basin and the highest known ground water level. Please provide a geotechnical report for review, and revise the site as necessary based on soils information.

Noted. We will include a summary of the groundwater elevations and depth below the bottom of the proposed basins once existing groundwater elevations have been obtained through completion of the geotechnical study.

SEPTIC SITES

1. Suitability of septic sites to be reviewed and approved by others.

Noted. We are working with Dale Denn of Homestead Septic on the design of the proposed septic systems.

WATER SUPPLY

1. Individual wells are proposed to provide water supply to the proposed residential lots and a community well is proposed to provide water supply to the car condos. Wells must meet requirements of the Minnesota Department of Health and applicable state laws and regulations.

Noted. We will process proposed well permit applications with the appropriate agencies.

OTHER ITEMS

1. Final Plans shall be in accordance with the approved Independence standards details and Engineering Manual. Final Plans shall be at a scale of 1" = 50' and include plan and profile sheets.

We will provide final plans in the required formats.

SUMMARY

At this time, there are several items as discussed herein which may affect the site layout. We recommend that the Applicant revise and resubmit prior to City consideration of the Preliminary Plat and Site Plan.

January 12, 2023

Mark Kaltsas
City Planner
City of Independence
1920 County Road 90
Independence, MN 55359

Lee Koppy
Anderson Engineering
13605 1st Avenue North, Suite 100
Plymouth, MN 55441

Bill Stoddard

SUBJECT: MnDOT Review #**S22-045**
Independence Business Park
SW Quad US 12 and Nelson Road
9285 Highway 12
Independence, Hennepin County

Dear Messrs. Kaltsas, Koppy and Stoddard:

Thank you for the opportunity to review the plans for the **Independence Business Park**. MnDOT has reviewed the documents and has the following comments:

Access to site:

In order to maintain safety on US 12, all access to the site should be obtained from County Line Road or Nelson Road. Reasonably convenient and suitable access to the property is available from Nelson Road. Providing access to either Nelson or County Line Road will allow drivers to access westbound US 12 without having to make a u-turn from the east or an illegal left turn from the right-out porkchop. Additionally, because of the number of trips predicted to be generated by the development, if the development accesses Nelson Road, MnDOT recommends a dedicated left turn lane from US 12..

For questions regarding these comments, please contact Eric Lauer-Hunt, Metro Traffic, at 651-234-7725 or eric.lauer-hunt@state.mn.us.

Pedestrian/Bicycle:

There is a planned bikeway on US 12 in the MnDOT Metro District Bike Plan. There is an existing trail running along County Line Road SE to the commercial center in the northwest corner

An equal opportunity employer

of County Line Road SE and US 12. MnDOT recommends the developer to leave space along US 12 for future trail connections.

For questions regarding these comments, contact Sara Dunlap, Metro Multimodal, at 651-234-7875 or eric.lauer-hunt@state.mn.us.

Water Resources:

The plans as submitted don't show the existing culvert at the proposed access to US12, nor does it show any proposed routing for this stormwater in the proposed condition. The developer will need to route stormwater and show no increase in flood levels in the MNDOT ROW as part of this project. Because of this, a MnDOT drainage permit will be required to ensure that current drainage rates to MnDOT right-of-way will not be increased. The actual HydroCAD models will need to be provided for review, not just project summaries.

The drainage permit application, including the information below, should be submitted online to: <https://dotapp7.dot.state.mn.us/OLPA/>

The following information must be submitted with the drainage permit application:

- 1) A grading plan showing existing and proposed contours.
- 2) Drainage area maps for the proposed project showing existing and proposed drainage areas. Any off-site areas that drain to the project area should also be included in the drainage area maps. The direction of flow for each drainage area must be indicated by arrows.
- 3) Drainage computations for pre and post construction conditions during the 2, 10, 50 and 100 year rain events.
- 4) Time of concentration calculations.
- 4) An electronic copy of any computer modeling used for the drainage computations.
- 5) See also the attached Drainage Permits Checklist for more information.

Once a drainage permit application is submitted, a thorough review will be completed and additional information may be requested.

Please direct questions concerning drainage issues to Jason Swenson, MnDOT Metro Water Resources at (651-234-7539) or Jason.Swenson@state.mn.us.

Permits:

As mentioned previously a Drainage permit will be required. Any other use of, or work within or affecting, MnDOT right of way will require a permit.

Permits can be applied for at this site: <https://olpa.dot.state.mn.us/OLPA/>. Please upload a copy of this letter when applying for any permits.

Please direct questions regarding permit requirements to Buck Craig of MnDOT's Metro Permits Section at 651-775-0405 or Buck.Craig@state.mn.us.

Review Submittal Options

MnDOT's goal is to complete reviews within 30 calendar days. Review materials received electronically can be processed more rapidly. Do not submit files via a cloud service or SharePoint link. In order of preference, review materials may be submitted as:

1. Email documents and plans in PDF format to metrodevreviews.dot@state.mn.us. Attachments may not exceed 20 megabytes per email. Documents can be zipped as well. If multiple emails are necessary, number each message.
2. PDF file(s) uploaded to MnDOT's external shared internet workspace site at: <https://mft.dot.state.mn.us/metrodevreviews.dot@state.mn.us>. Contact MnDOT Planning development review staff at for uploading instructions and send an email listing the file name(s) after the document(s) has/have been uploaded.

If you have any questions concerning this review, please contact me at (651) 234-7797.

Sincerely,



Cameron Muhic
Senior Planner

Copy sent via E-Mail:

Buck Craig, Permits
Jason Swenson, Water Resources
Aaron Tag, Area Engineer
Sara Dunlap Metro Multimodal
Kimberly Zlimen, Transit
Tom Cruikshank, District 3 Planning
Bethany Brandt-Sargent, Metropolitan Council

Lance Schowalter, Design
Eric Lauer-Hunt, Traffic
Doug Nelson, Right-of-Way
Michael Kowski, Maintenance
Brandon JB Nelson, Surveys
Natalie Ries, Noise
Jed Hanson, Metropolitan Council